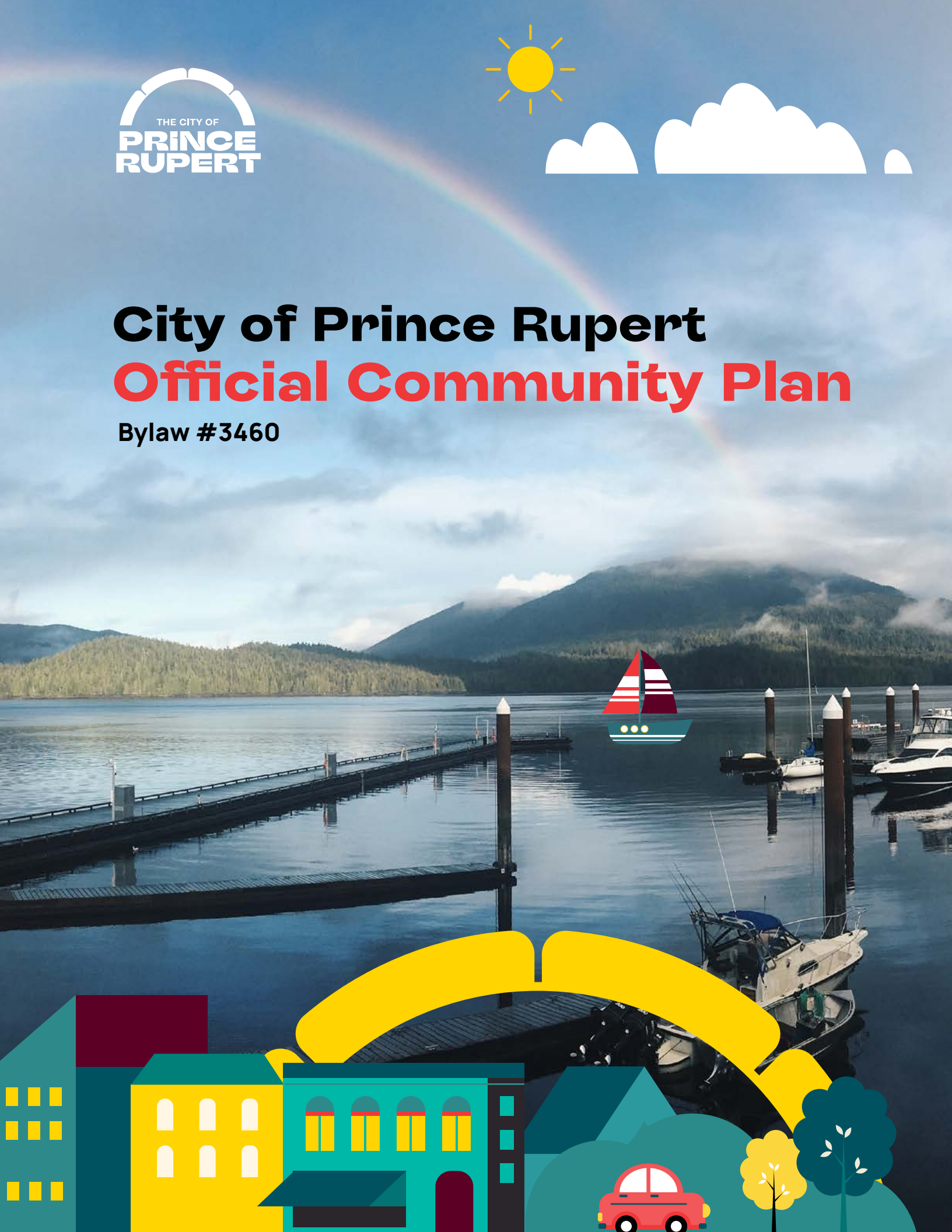
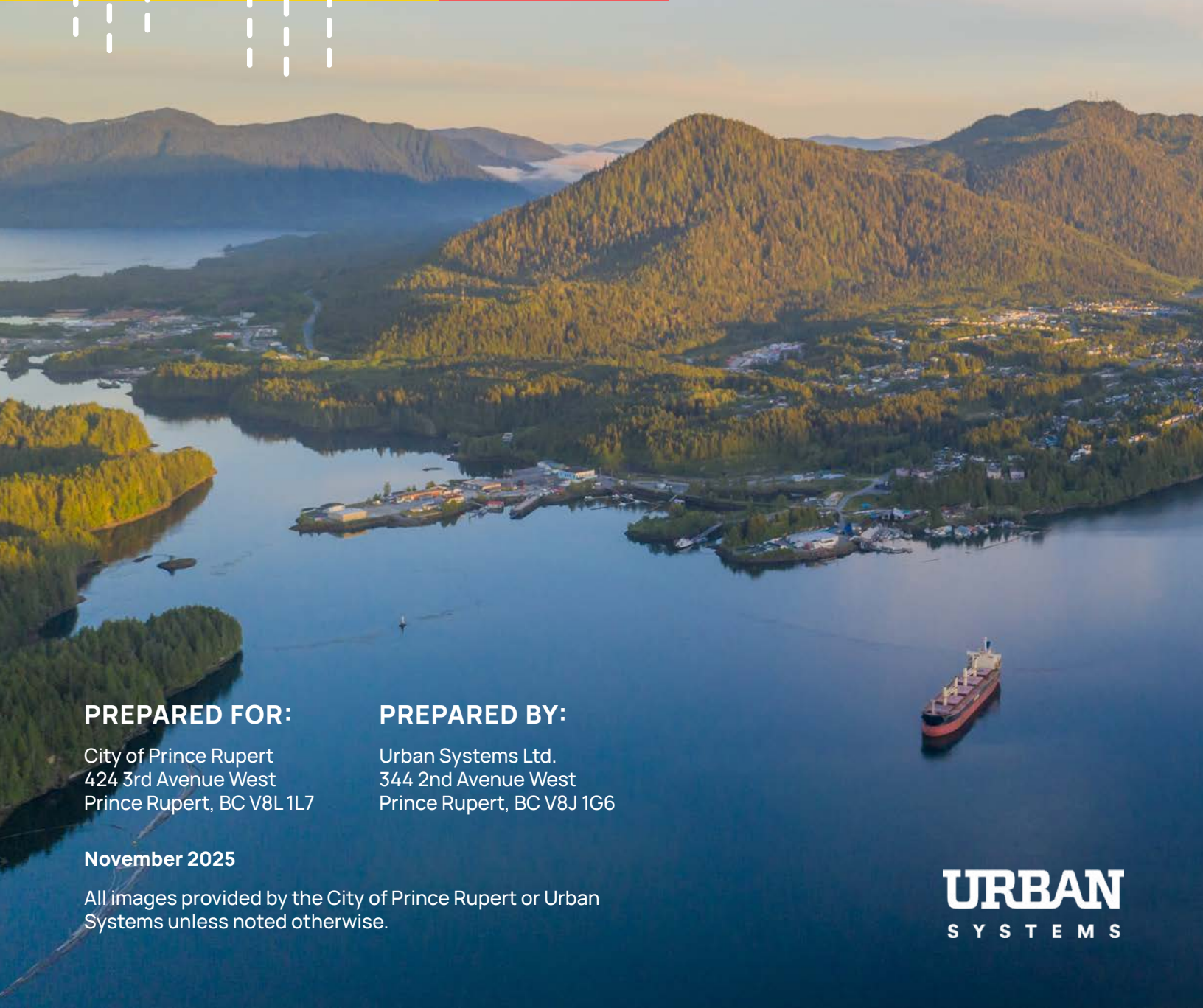
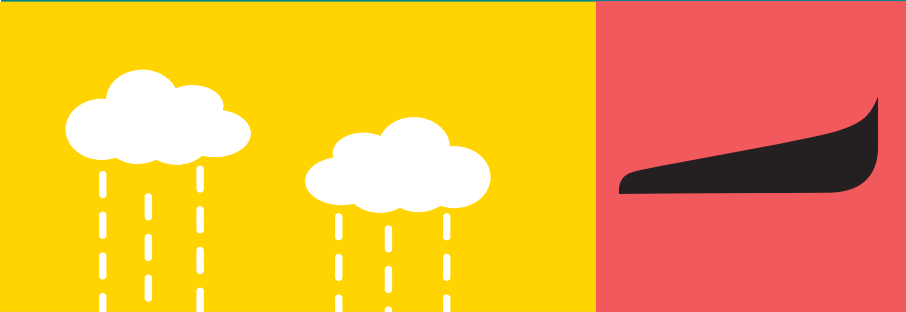




# City of Prince Rupert Official Community Plan

Bylaw #3460





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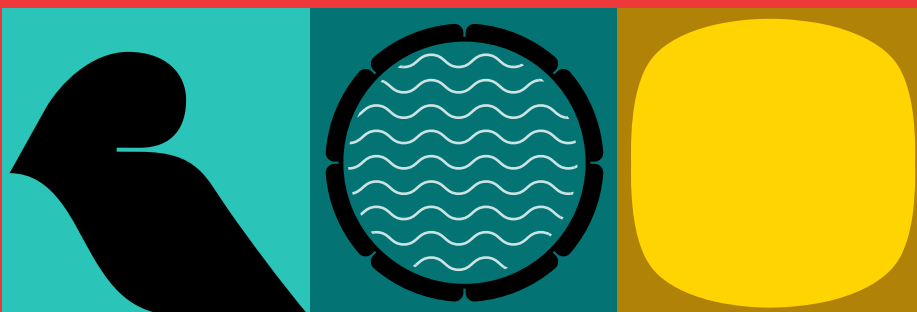
**November 2025**

All images provided by the City of Prince Rupert or Urban Systems unless noted otherwise.



## TERRITORIAL ACKNOWLEDGMENT

We respectfully acknowledge that what is now known as Prince Rupert is the traditional, ancestral, unceded territory of the Ts'msyen people. There are recommendations in this plan that can be undertaken by the City as part of Truth and Reconciliation. This plan is by no means the answer to meaningful reconciliation, but it is intended to promote and support the continued dialogue to learn from and engage with Indigenous partners.







# OUR VISION

Prince Rupert, a World-Class Port City, is:

VIBRANT

RESILIENT

SUSTAINABLE

PROSPEROUS

ACTIVE AND VITAL

SAFE AND BEAUTIFUL

HEALTHY AND ACTIVE

PROUD OF ITS HERITAGE

INCLUSIVE AND EQUITABLE

PROUD OF ITS COMMUNITY

NESTLED CAREFULLY IN NATURE

A PLACE THAT PEOPLE WANT TO BE



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# INTRODUCTION

The OCP is an important bylaw that describes the long-term vision of community and land development in Prince Rupert.





The Official Community Plan (OCP) is a cornerstone document of any community and represents its proactive effort to plan for the future. As a community, Prince Rupert already has an excellent track record of setting bold visions for its near and long-term development. However, these visions have been challenged in recent years as critical failures in the city’s infrastructure systems have resulted in large unforeseen costs and attention. In response to these challenges, the City has undertaken careful and detailed infrastructure reporting to achieve millions of dollars of new funding in 2023 (\$65 million) and 2024 (\$77 million). In addition to successes in planning for the city’s underlying infrastructure, the City has also been proactive in improving its transportation, parks, and social systems through various other plans and policy updates. These efforts continue with important projects on climate change, complete communities, and others.

These initiatives form the foundation for the City’s 2025 Official Community Plan update.

This Official Community Plan, updated in 2025, continues to reflect the community vision developed by the Prince Rupert 2030 Vision strategy (The 2030 Vision) and the community’s desired direction identified in the 2021 OCP engagement process. The 2025 targeted update of this OCP includes reference to new regulations from the Province, incorporates relevant outputs from recent plans and policies, and adds action-oriented language to achieve the community’s vision.

Prince Rupert endured a population decline, along with the resulting challenges created for the local economy and health of the downtown and residential areas, since the 1980s, but now faces the real prospect of economic and population growth. How the City manages this growth will profoundly affect its attractiveness

for new residents and businesses. This OCP sets out a framework for the city’s future and principles for growth management that will encourage quality of life and vitality, bolster the pride and affection residents have for this city, and encourage new residents and businesses to make Prince Rupert their home.

This OCP recognizes that the quality and vitality of the city are linked symbiotically to the health of the port industry. If the new workforce required to service port growth is not attracted to live in Prince Rupert, the port will have significant challenges securing the human resources required for its operations. In other words, building and maintaining an attractive and vibrant city with the services required and desired by residents is a fundamental part of the port’s success – a quality downtown and attractive residential areas support economic growth.



## 1.1 LEGISLATIVE FRAMEWORK AND AUTHORITY

Once an OCP is adopted as a bylaw, all future decisions (e.g., zoning, park development, construction of City buildings, and road development initiatives) must be consistent with the objectives and policies outlined in the plan. An OCP is principally a policy document which does not compel Council to undertake any specific project.

The effective implementation of an OCP requires several tools. These include using regulations and guidelines such as the zoning bylaw, development permit area guidelines, planning for capital expenditures, and road construction standards bylaws. These bylaws and regulations must be consistent with the principles and policies of the plan. Recognizing that circumstances can change over time (e.g., community values, environmental, and economic conditions), the provincial legislation that directs the development of OCPs provides Council the ability to amend the OCP, subject to a public hearing, to allow the community to have input into the proposed amendment.

In determining the content of the OCP, the City is guided by the Local Government Act's (LGA) definition of the purpose of municipal government. This legislation establishes a broad purpose that allows for a municipality to:

- Provide for the good government of its community
- Provide for services, laws, and other matters for community benefit
- Provide for stewardship of the public assets of its community
- Foster the economic, social and environmental well-being of its community

In addition to the broadly defined municipal purpose, the LGA defines specific content and process requirements for developing a new Official Community Plan. These are reviewed in the next sections.

### MANDATORY OCP CONTENT

The Local Government Act requires that an OCP must have certain statements and map designations for the following matters:

- a. Residential development required to meet anticipated housing needs over a period of at least 20 years, as determined by the community's Housing Needs Report
- b. The approximate location, amount and type of present and proposed commercial, industrial, institutional, agricultural, recreational, and public utility land uses
- c. The approximate location and area of sand and gravel deposits that are suitable for future sand and gravel extraction
- d. Restrictions on the use of land that is subject to hazardous conditions or that is environmentally sensitive to development
- e. The approximate location and phasing of any major road, sewer, and water systems
- f. The approximate location and type of present and proposed public facilities, including schools, parks, and waste treatment and disposal sites



The LGA also requires an official community plan to include:

- a. Housing policies of the local government respecting affordable housing, rental housing, and housing for people with other specific needs
- b. Targets for reducing greenhouse gas emissions in the area covered by the plan, along with the policies and actions of the local government that are proposed to achieve those targets

Without the above mandatory plan elements, the plan would not qualify as an Official Community Plan.

## OPTIONAL OCP CONTENT

In addition to the mandatory plan elements, and consistent with the broad range of municipal purposes, the Local Government Act allows municipalities to include other policy matters in their OCPs as follows:

- a. Policies of the local government relating to social needs, social well-being, and social development
- b. A regional context statement if there is a Regional Growth Strategy (RGS) in place for the region (Note: there is no RGS in place for the North Coast Regional District)
- c. Policies of the local government respecting the maintenance and enhancement of farming on land in a farming area or in an area designated for agricultural use in the community plan
- d. Policies of the local government relating to the preservation, protection, restoration and enhancement of the natural environment, its ecosystems and biological diversity

With respect to matters that are not within the jurisdiction of the City, the provincial legislation provides that a Council can have policy statements regarding those matters, but that such statements can only be made as broad objectives of the Council unless the Province provides specific permission to have more detailed and specific policy.

## ENGAGEMENT

Engagement with the residents and stakeholders in a community is a fundamental part of preparing an OCP. This is reflected in the Local Government Act's requirement that during the development or amendment of an OCP, the City must provide at least one opportunity for consultation with people, organizations, and authorities that the City considers will be affected by the OCP. When this plan was updated in 2021, the City chose to undertake early, ongoing, and broad consultations, including, but not limited to, the following:

- a. Prince Rupert Residents
- b. Neighbouring First Nations
- c. Prince Rupert Port Authority
- d. Prince Rupert School District
- e. Community Futures of the Pacific Northwest
- f. North Coast Regional District
- g. District of Port Edward
- h. Non-Profit Organizations

The 2021 OCP update, which forms the foundation of this document, built on the 2030 Vision by affirming its core values and goals. The engagement process for the 2021 OCP update was designed to ensure broad participation and incorporate diverse inputs from various sources and events. In 2021, the public engagement confirmed strong



community support with 617 site visits and 125 survey responses overwhelmingly endorsing the 2030 Vision's core elements. This new OCP update recognizes the importance of retaining valuable elements from earlier planning efforts.

In 2025, Prince Rupert undertook minor updates to the OCP to comply with new regulations from the Province and incorporate relevant outputs from the City's recent plans and policies.

## OTHER LEGISLATION

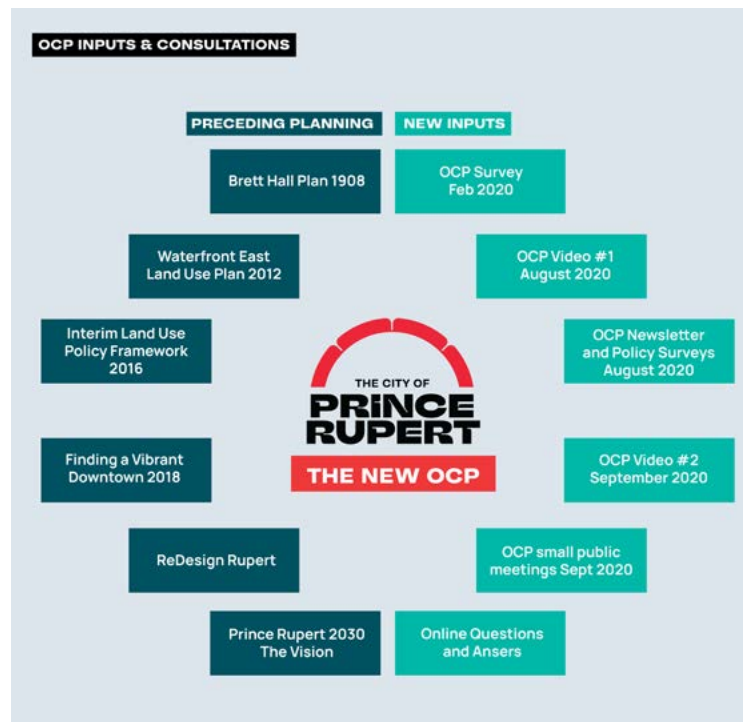
In addition to the guidance of the Province of British Columbia's Local Government Act, the City is also affected by other legislation.

Federal and provincial statutes and regulations impact the business of the City of Prince Rupert. Some of the more notable statutes and regulations include the Riparian Areas Protection Act (governing development in riparian areas), the Highway Act (governing provincial highways), the Canada Transportation Act (governing railways), the Canada Marine Act (governing ports), the Aeronautics Act (governing airports), and the Environmental Management Act (governing sewage disposal).

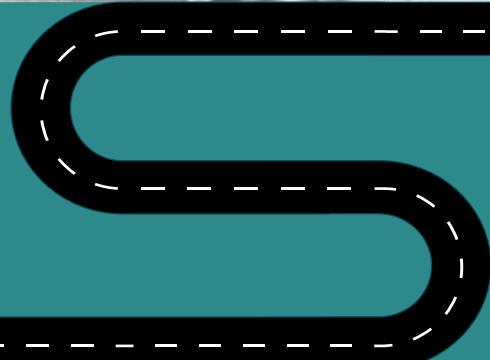
In 2023, the Province of British Columbia introduced Bill 44 – 2023: Housing Statutes (Residential Development Amendment Act, 2023), which had several transformative impacts on land use planning and residential development in Prince Rupert. Bill 44 had two focuses:

1. **Small-Scale Multi-Unit Housing (SSMUH):** Mandated minimum allowable densities for residential areas. To comply, Prince Rupert had to update its zoning bylaw to permit small-scale multi-unit housing in its low density residential zones.

2. **Proactive Planning:** Bill 44 established a new standardized approach to calculating housing needs and requires municipalities to prepare an interim Housing Needs Report and update their OCP to ensure it can accommodate the community's 20-year housing needs. Bill 44 also phases out one-off and site-by-site public hearings for housing project rezonings consistent with OCPs that have already had a public hearing. Public notice is still mandatory for projects that do not hold public hearings. To comply, Prince Rupert prepared an updated Housing Needs Report in late 2024 and has confirmed the capacity to deliver to accommodate the City's total housing need in this updated OCP.



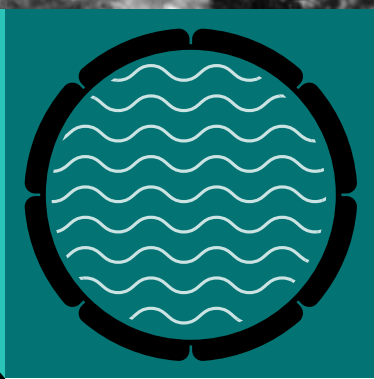
OCP INPUTS  
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# BACKGROUND

The history of Prince Rupert and the surrounding area is rich and extends much further back in time than the date of incorporation in 1910. Since time immemorial, Kaien Island and the outlying area have been the home of the Ts'msyen. Nestled into the mountain shadow, our community is well known for rain, which gives us year-round greenery and abundance. In addition, our deep natural waters have made us a strategic location for trade – now, as in the time of the Grease Trails.

AVE. PRINCE RUPERT B C





A VIEW OF PRINCE RUPERT FROM THE WEATHER STATION LOCATED ON TOP OF MOUNT HAYS

## 2.1 GEOGRAPHIC CONTEXT

Affectionately known as the City of Rainbows, Prince Rupert is the largest community on the northwest coast of British Columbia. It is located on Kaien Island, about 770 kilometres northwest of Vancouver and only 90 kilometres from the Alaskan Panhandle. The island is the western terminus of the Trans-Canada (Yellowhead) Highway 16 and the Canadian National Railway, and is linked to the mainland by a short bridge. It is 144 kilometres west of Terrace and 715 kilometres west of Prince George.

The majority of the city is on the coast at the base of Mount Hays, which rises 732 metres above sea level. The city boundaries also include the comparatively level Ridley Island, Watson Island and an area of land known as District Lot 444, which is situated on the mainland across Fern Passage to the north. Kaien, Ridley, and Watson Islands are characterized by underlying bedrock covered by organic soils and muskeg ranging from less than one meter to more than eight metres in

depth. The muskeg conditions pose a challenge and expense for land development.

The southern streams on Kaien Island support Cutthroat trout and Dolly Varden. Hays Creek in the northwest townsite area also supports Pink and Coho salmon. The nearby Skeena River and Ocean waters support salmon, halibut and herring fish as well as a number of other marine species.

Because of the area's abundant precipitation, combined with its location on the coast and amid coastal mountains, Prince Rupert enjoys a majestic natural setting characterized by landscapes of green and blue. This palette is periodically accented with red, orange, yellow, green, blue, indigo and violet displays in the rainbows that share their colours over Prince Rupert. The area's vegetation is indicative of the Montane zone, which includes Mountain Hemlock, Red Cedar, Alder, Pine, and Sitka Spruce.



1907 BRETT AND HALL PLAN (PRINCE RUPERT'S ORIGINAL TOWNSITE PLAN/MAP)

## 2.2 HISTORICAL CONTEXT

This new Official Community Plan is not the first OCP or plan prepared for Prince Rupert. Indeed, planning in Prince Rupert began with its initial conception in the 1907 Brett and Hall plan (see Figure 1), which laid out a vision of beautiful crescents, human-scaled blocks, and an efficient street grid in the City Beautiful tradition of planning, which was the leading planning model of the time. While several community plans have been prepared for the city since 1907, the Brett and Hall plan structure endured up to the 2007 OCP and was embraced by the 2021 OCP update.

The 2025 OCP update continues to respect and agree with the Brett and Hall plan layout. It is also informed by the previous OCPs and subsequent planning work including, for example, The Water Front East Land Use Plan (2012), the Interim Land Use Policy Framework (2016), Finding a Vibrant Downtown (2018), and the Prince Rupert 2030 Vision document. Developing an OCP should be seen as an evolutionary process. It emerges out of a

context and in a community with a long history; the context is rich, having benefited from the sharing and melding of knowledge from the local population with professional expertise. The heritage of trade as a foundational economic activity in Prince Rupert did not begin with the initial Brett and Hall Plan. Prior to European settlement, Kaien Island was a significant location for Aboriginal trade. Prince Rupert's establishment and the arrival of the rail line continued the land use tradition of the Ts'msyen First Nations.

A plan is more meaningful and valuable if it builds on and recognizes all of the past energy and values invested and found in the previous work and development. It also necessarily looks forward to and guides the next steps in the community's growth, considering new realities of local, regional and global influences (such as trade, climate change, and public values) as they impact the City's potential with constraints and opportunities.



**CONTAINER VOLUMES HAVE STEADILY GROWN SINCE THE OPENING OF THE CONTAINER FACILITY IN 2007, IN ADDITION TO OTHER EXPORT-BASED DEVELOPMENTS.**

.....

The City is within reach of realizing its long-held aspirations for full development as a vital, prosperous port city. To be clear, there have been very positive economic times for the City in its 110-year history as an incorporated city, with the boom of the fishing and forestry sectors. There have also been times when prosperity stalled because of the decline of those sectors. Prince Rupert's census population reached a high of 16,714 in 1996 but declined to 12,220 in 2016 (Census Canada). During the 1990s, the population would swell significantly in the summer due to the seasonal fishing industry, making the 16,000 estimate on the lower end of the actual population. According to Census Canada, the population decline slowed from 2011 to 2016, with a decline of only 288 people. It should be noted that the actual population during these census periods was probably higher, as the census methodology does not effectively capture the entire population.

At this time in the history of Prince Rupert, the original vision of the city becoming a key and bustling port serving the Pacific trade region promises to be realized due to the expected rapid expansion of the port facilities and the growth and prosperity that can be generated from that expansion. The Prince Rupert 2030 vision document was prepared as a specific response to the promise of port development, and it heavily influences and guides this plan. It is now anticipated that the Prince Rupert population will increase significantly as port expansions bring in new workers for the port and the construction of new facilities, workforce housing, and service and retail businesses required for the new workers.

Growth that adds considerably to the local quality of life for residents and neighbouring First Nation communities will take considerable effort and collaboration. The expected boom the port generates can be positive or negative,



depending on how it is managed and directed. This speaks to the need for preparation, resources, communication, collaboration, and difficult decision-making by all parties involved in the development of a community; topics that will be further addressed in the section on implementation.

## 2.3 REDESIGN RUPERT 2030 VISION

This new OCP builds on preceding work, including the Prince Rupert 2030: The Vision strategy (the 2030 Vision). The 2030 Vision Document was the culmination of work that began in 2015 and continued to 2019, when Redesign Rupert engaged residents to determine their needs and wants for the community.

The 2030 Vision presents an ambitious aspiration for the community. It is presented as a vision that will inevitably be different in its details when achieved. This advice is given by the author of the strategy to help focus on the broader vision rather than becoming overly focused on details. In other words, there will need to be some acceptance and flexibility in how the vision is realized. This is especially true because implementation will involve additional processes, including this OCP, where residents and stakeholders will have opportunities for receiving further information and providing input based on plan aspirations, ongoing municipal operations, projects, industry investments, and available funding, resources, and capacities.

## 2.4 FIRST NATIONS

It is impossible to understand the history of Prince Rupert without acknowledging the people who inhabited this land long before contact with Europeans and before Prince



**THE 2030 VISION DOCUMENT WAS RELEASED IN DECEMBER OF 2019, FOLLOWING MULTIPLE PUBLIC ENGAGEMENTS AND A MAJOR UNVEILING EVENT.**

Rupert was incorporated. British Columbia comprises dozens of diverse Indigenous groups with distinct languages, cultures, and histories. In Prince Rupert, it is the Ts'msyen nations of Lax Kw'alaams and Metlakatla whose territory we reside on.

The Ts'msyen people are the descendants of the Nine Tribes of the Ts'msyen who have lived in their territories since time immemorial. In defence of invading Tlingit from the north about 2000 years ago, the Nine Tribes consolidated their efforts and became one of the region's most powerful political, economic, and military powers. In terms of trade, the Nine Tribes influence extended along the North Coast and into the interior as far as Kispiox, following the Grease Trails used for transporting Eulachon. At the time of contact in the 1790s, when the fur trade brought settlers to the BC coastline, the Nine Tribes were a significant source of power on the North Coast.

In 1834, what was then called Fort Simpson was built at a site known as Lax Kw'alaams (Place of the Wild Roses), where the reserve community continues to be located. This site was owned by a dynasty of Chiefs from the Gispaxlo'ots tribe (of the Nine Tribes) known as the Legex. For the use of his territory, Legex imposed a fee on anyone who wanted to trade at the fort.



**PICTOGRAPH MARKING THE TERRITORY OF THE HOUSE OF LEGEX ALONG THE SKEENA RIVER**  
.....

A painting indicating the territory of Legex is still visible on a rock wall at the mouth of the Skeena.

The Metlakatla First Nation, meanwhile, is located about five kilometres north of Prince Rupert on an ancient site occupied by the Ts'msyen for thousands of years. Metlakatla means "Saltwater Pass" in Sm'algyax, which is the language of the Ts'msyen people. Historically, the site was the collective winter village of the Nine Tribes of the lower Skeena River.

Post-contact, despite the impacts of colonization and the residential school system, the Ts'msyen continue to maintain a strong presence in Prince Rupert and are a vital part of our social and cultural fabric. Sm'algyax is now taught in the local School District, and the Ts'msyen and Indigenous People generally in Prince Rupert continue to practice traditional customs, food gathering and preparation, and learn their languages and histories. In addition to Ts'msyen people, Prince Rupert is also home to a diaspora of Indigenous Peoples from the surrounding areas – including Nisga'a, Haida, Gitksan, Haisla, and Tlingit, among others. Approximately half of the population of Prince Rupert is Indigenous, and Indigenous Peoples, as well as Prince Rupert residents in general,

also come together to celebrate events like National Indigenous Peoples Day, Salmon Fest, and, of course, the All Native Basketball Tournament.

## 2.5 POPULATION GROWTH AND HOUSING NEEDS

Prince Rupert considered population growth when developing residential, commercial, industrial, and other land use policies. Projecting population growth involves many uncertain factors such as migration, fertility, and the economic climate. In 2024, the Province of British Columbia established a new standardized approach to calculate housing need, which must be reflected in a community's Official Community Plan. This standardized approach uses an average of local and regional household growth projections from BC Stats to project growth.<sup>1</sup>

The Province of British Columbia's Housing Needs methodology estimates that the City of Prince Rupert will need 1,289 new housing units over the next 20 years (see Table 1).

.....

1 Please note that the Housing Need methodology developed by the Province likely underrepresents the total housing need in Prince Rupert. Given changes to homelessness counts and vacancy data since 2021, as well as the lack of consideration for any anticipated changes to population growth patterns despite major investment and expansion of the port industries, the provincial methodology for estimating housing need over the next five and 20 years could underestimate actual housing need by over 200 units with existing data, and likely another several hundred with anticipated growth from the Prince Rupert Port. For more information, please refer to the City of Prince Rupert's [Interim Housing Needs Report](#).

**TABLE 1. TOTAL 5-YEAR AND 20-YEAR HOUSING NEED, CITY OF PRINCE RUPERT, 2021 – 2041**

| <b>Individual Components of Provincial Housing Need Methodology</b>                       | <b>5-Year Need<br/>(2021 - 2026)</b> | <b>20-Year Need<br/>(2021 - 2041)</b> |
|---|--------------------------------------|---------------------------------------|
| <b>Component A: Supply of Units to Reduce Extreme Core Housing Need</b>                   | 43                                   | 171                                   |
| <b>Component B: Supply of Units to Reduce Homelessness</b>                                | 67                                   | 135                                   |
| <b>Component C: Supply of Units to Reduce Suppressed Household Formation</b>              | 62                                   | 246                                   |
| <b>Component D: Supply of Units to Meet Household Growth Over the Next 5 and 20 Years</b> | 293                                  | 467                                   |
| <b>Component E: Supply of Units Needed to Meet at Least a 3% Vacancy Rate</b>             | 0                                    | 0                                     |
| <b>Component F: Supply of Units Needed to Meet Local Demand (Demand Buffer)</b>           | 67                                   | 269                                   |
| <b>Total Units Needed</b>   | <b>532</b>                           | <b>1,289</b>                          |

Source: Housing Assessment Resource Tools, 2024



The City is also required to ensure the OCP has enough capacity to accommodate the 1,289 housing units. Population growth and resulting housing development will depend on several factors outside the City's control, including local economic conditions, developer interest, local trades capacity, and senior government funding. This includes projections for port expansion and the associated population growth outlined in the 2030 Vision, which anticipates a significantly higher growth rate than the estimates produced using the provincial methodology. This OCP ensures land-use patterns and capacity anticipate the known need for housing and the potential increase to ensure housing remains accessible.

Through an analysis of the OCP's land use capacity for new housing, Prince Rupert confirmed there are several opportunities by which the current OCP can accommodate the 1,289 housing units:

- **Existing Small-Scale Homes:** There are currently 2,950 single-family dwelling units. If each were to add a secondary suite or Accessory Dwelling Unit (ADU), the City could increase its housing stock by up to 2,950 units. While not all single-family homes will integrate a suite or ADU, this figure reflects the available regulatory capacity, which is well in excess of what is currently required by the Province.
- **Future Neighbourhoods:** [Map 1B](#) identifies Future Greenfield Neighbourhoods in Prince Rupert, which can accommodate future housing development. Excluding known development constraints, this area has the opportunity to accommodate approximately 213 to 1,065 new housing units, depending on the density.



# PLANNING PRINCIPLES

This OCP is based on and guided by a number of broad and specific planning principles. These principles are used to guide the policies for the future development of the City of Prince Rupert, focusing on long-term livability and economic success. Many of these principles overlap with shared goals of community-wide health, long-term livability and a vibrant, walkable community. The broad planning principles include: Sustainable Cities, Smart Growth, Healthy Communities, Complete Communities, and Engagement and Consultation.





### 3.1 SUSTAINABLE CITIES

The 1987 Brundtland Report, commissioned by the United Nations, presented the argument for sustainable development, which it defined as “meeting the needs of the present without compromising the ability of future generations to meet their own needs.” At a fundamental level, sustainability can be defined as “able to be sustained,” where the root “sustain” means to keep something going continuously. Sustainability speaks to a state where we stay within our means or natural income. In comparison, resilience speaks to the capacity of a system to absorb shocks and maintain function. The latter emphasizes natural and social diversity as a characteristic of high resiliency. A sustainable city prioritizes policy that maintains economic growth while creating a livable city, focusing on resource efficiency, cleanliness, and green technologies. In developing a sustainable city, partnerships must be facilitated between the community, the public, and the private sectors. The vision of sustainable development requires that resource efficiency be met through responsible resource consumption and production. This can be achieved through the principles of a circular economy (reduce, reuse, recycle) and efficient use of green energy technologies. Following these principles will encourage clean, green, and healthy cities with a reduced ecological footprint, ensuring the long-term success of a city and its economy.

### 3.2 SMART GROWTH

Smart growth guides the planning process with principles that promote walkable communities, compact development, and mixed land uses. Smart growth policy encourages a range of housing opportunities, including single-use and multi-family housing, to increase neighbourhood density and housing diversity while maintaining the community character.

Dense, walkable neighbourhoods support local commercial centers. Smart growth encourages compact community design to protect wildlife within the community and create walkable neighbourhoods with retail and other services. Smart growth principles can help neighbourhoods achieve a strong sense of community and place, while increasing accessibility of housing, transportation, and employment. Prince Rupert’s future vision incorporates smart growth principles to enhance existing neighbourhoods and provide options for housing, transportation, and amenity services within a walkable distance.


### 3.3 HEALTHY COMMUNITIES

A healthy community is a general concept defined as “a place where healthy built, social, economic, and natural environments give citizens the opportunity to live to their full potential” (Canadian Institute of Planners,



THE MARINA DISTRICT ENCOMPASSES THE CURRENT COW BAY DISTRICT, A BUSTLING COMMERCIAL AREA OF PRINCE RUPERT.





2019). A healthy community policy addresses the built environment, the natural and rural environments, and the social environment. Like smart growth, green spaces are provided throughout the community to ensure proximity to all residents and promote a walkable community and physical activity. Healthy community design increases social connection through reinforcing infrastructure such as parks and trails, creating a stronger sense of community. Community centres are placed within communities, and social events are desired. Accessible housing for all community members is a critical goal in healthy communities. A healthy community policy encourages affordable housing and accessible community amenities to produce a socially inclusive and cohesive community. The healthy community approach requires consultation and engagement with Indigenous and non-Indigenous communities, the private sector, and the general public to address diverse perspectives, needs, and concerns.

### **3.4 COMPLETE COMMUNITIES**

A complete community provides diverse housing to meet identified community needs, accommodate people at all stages of life and abilities, and provide a wider range of employment opportunities, amenities, and services within a walkable distance. Creating a more complete community can support a range of identified community goals and offer many interrelated benefits, including more housing and transportation options, increased walkability, accessibility, age-friendliness, and equity, greater efficiency with servicing and infrastructure, environmental sustainability, and preservation of the natural environment by reducing urban sprawl.

In 2024, the City of Prince Rupert undertook a Complete Community Assessment (CCA) through funding from the Union of BC Municipalities. The CCA analyzed the city through four key community lenses (Infrastructure, Transportation, Access to Daily Needs, and Housing) to measure community completeness and walkability levels. The key outcomes of this work included a detailed assessment of walkability in Prince Rupert and new suggested street cross sections for improved walkability and accessibility throughout the community. This report enables the City to make data-driven decisions on implementing the pedestrian network for all ages and abilities envisioned in the Connect Rupert Transportation Plan.

### **3.5 ENGAGEMENT AND CONSULTATION**

It is a core principle that city planning includes meaningful engagement and consultation with all stakeholders. A collaborative approach between stakeholders, councils, and citizens is essential to strengthening relationships and creating diverse and inclusive community plans. Participation can occur at many levels within the community, from creating solutions to identifying needs to planning future projects. Effective engagement allows policymakers and elected officials to recognize the community's diversity of needs and aspirations.



## 3.6 OUR PLANNING PRINCIPLES

In addition to the above principles, more focused principles guide this OCP. Regarding principles for the successful renewal of Prince Rupert, the 2030 Vision document proposes five principles:

- Growth and change in Prince Rupert must enhance community well-being.
- Growth and development in Prince Rupert must enhance community sustainability and resilience.
- Growth and development in Prince Rupert must add or enhance unique local character in tune with the special natural setting.
- Growth and change in Prince Rupert must embrace diversity.
- Growth and development in Prince Rupert must facilitate communities/ port mutual supports, commitments, and accords.

During engagement for the 2021 OCP update, these principles were brought back to the community which confirmed alignment and support.

# LAND USE POLICIES

Official Community Plans include designations that describe permissible uses within different community areas. The primary designations in Prince Rupert's OCP are Residential, City Core, Commercial, Industrial, and Parks and Recreation. Each category of land use has accompanying policies that dictate how lands are used in those areas.





**THIS RESIDENTIAL AREA IS JUST ABOVE RUSHBROOK AND OVERLOOKS THE PRINCE RUPERT HARBOUR.**

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This Official Community Plan designates the lands within the city's boundaries for a variety of land uses on [Map 1A \(City-Wide\)](#) and [1B \(Townsite\)](#). [Maps 1A](#) and [1B](#) identify three land use areas in the City Core's Midtown, Downtown, and Marina Districts. Permitted uses in each of these areas are as follows:

- a. The Midtown District allows mixed-use, residential, restaurants, cafes, hotels, retail, offices, institutional, and personal services.
- b. The Downtown District allows mixed-use, residential, retail, restaurants, cafes, offices, institutional, hotel, traveller accommodations, port uses, marine uses, live-work units, studios, galleries, distillery, and personal services.
- c. The Marina District permits mixed-use, residential, retail, restaurants, cafes, traveller accommodations, marine, galleries, institutional, live-work units, light industrial manufacturing, tourist commercial, technology development, studios, recreational uses, distillery, and personal services.

In addition to the specific uses identified in [Map 1A](#), urban agricultural uses are allowed, subject to appropriate zoning and regulation.

These three areas have a broad range of designated land uses within their boundaries. [Map 1C](#) represents the 2030 Vision conceptual land use framework and presents one possible distribution of these land uses. This map does not designate land uses but serves as a guide to the type and pattern of uses intended to achieve this plan's broad goals. During the finalization of the 2030 Vision, it was emphasized that the fine-grained land use patterns in that strategy were conceptual and likely to vary over time. It is presented here as a desired pattern of land uses to assist Council and the community in assessing any future rezoning and land development proposals in the City Core. Proposals that vary from [Map 1C](#) but are consistent with [Maps 1A](#) and [1B](#) are considered consistent with the OCP. Council, however, can consider the merits of each rezoning application in terms of several factors, including, for example, fit with [Map 1C](#), servicing issues, public input, and compliance with the other policies of this OCP.

The following sections in this chapter speak to the details and policy of each land use designation.



**THE MARINA DISTRICT ENCOMPASSES THE CURRENT COW BAY DISTRICT, A BUSTLING COMMERCIAL AREA OF PRINCE RUPERT.**

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THE CEDAR VILLAGE SENIORS' HOUSING FACILITY IS A COLOURFUL ADDITION TO 7TH AVENUE EAST, COMPLETE WITH A TRADITIONAL CEDAR WOVEN HAT FEATURE AND COMMON AREAS FOR GATHERING.

## 4.1 RESIDENTIAL

Lands designated as General Residential (detached, townhouse, or apartment) and Future Greenfield Neighbourhoods on [Maps 1A](#) and [1B](#) allow residential uses. While the [Zoning Bylaw](#) shall determine the density and form, the residential areas outside of the Downtown District shall be principally characterized by detached residential uses, along with some well-designed, integrated, and scaled multi-family forms of housing that blend into neighbourhoods. The City Core area includes residential designations allowing townhouse and apartment housing forms. It also includes mixed-use retail designations, allowing residential uses above commercial uses. In the design and approval of greenfield sites, care should be taken to provide adequate buffering from residential areas to any adjacent industrial uses.

Having housing stock that meets the community's diverse needs is a foundation upon which the health and prosperity of the city depend. The type of housing provided needs to address, for example, the differing capacity to afford homes, sufficient room for families, places to comfortably grow old in, places to start as an individual or couple, support for those with special needs, and places that are close to services and amenities.

Housing is also a core concern of industry. If there is no housing available that meets the needs of new workers to service industry expansion, industry will have a very difficult time expanding, limiting economic growth. To address the need for diverse, affordable, attractive, and inviting housing, the City and its partners will need to work together to address the community's housing needs. The City has outlined its commitments to addressing housing challenges in the Housing Acceleration Action Plan.

While this OCP only applies to the lands within the City of Prince Rupert, it is acknowledged that there will be housing units outside the city boundaries and that they are not subject to the plan for First Nations' communities shared among each of the First Nations.

### RESIDENTIAL POLICY

1. Pursue the commitments made in the Housing Acceleration Action Plan to address the barriers to market and non-market housing development to meet the City's 20-year housing need of 1,289 new units.
2. Monitor local economic conditions and development trends to ensure infrastructure, servicing, and land use capacity can accommodate population growth that may exceed current projections.



3. Direct most of the new residential growth to existing neighbourhoods and areas designated as Future Greenfield Neighbourhoods in this plan. Multi-family residential, including supportive housing developments, will also be directed to all commercial zones.
4. Prioritize the development of affordable housing, including housing for families, renters, seniors, workers, people living with complex mental health challenges and acquired brain injuries, and intergenerational households.
5. Encourage new childcare spaces as part of multi-family housing development.
6. Regularly review the Business Regulation and Licensing (Rental Units) Bylaw to ensure it can protect renters and manufactured home park residents from the impact of “reno-victions.”
7. Allow affordable multi-unit housing, defined as housing providing non-market affordable or special needs rental housing units secured by covenant, to exceed OCP density limits. The density limits that can be exceeded include the framework for building heights in the City Core area (see [Map 2](#)).
8. Encourage a variety of housing typologies and tenures that are designed to blend into existing neighbourhoods, including:
  - a. Small-scale multi-unit housing in existing residential neighbourhoods
  - b. Multi-unit development in walkable areas
  - c. Rental housing development in higher-density areas through multi-unit development and in low density areas through secondary suites and accessory dwelling units
  - d. Short-stay strata-titled apartments in the downtown core
  - e. A mix of small-scale multi-unit housing in the Midtown District near Five Corners
  - f. Mixed-use development in the Marina District east of Cow Bay
  - g. Non-market units throughout the city controlled by statutory housing agreements, covenants, or both
9. Implement new housing actions in collaboration with local partners as the community grows, including:
  - a. Establishing a Community Housing Corporation to assist in finding resources, funding, partnering, encouraging, and promoting the type and quality of housing needed for the city to realize its potential as a world-class port city.
  - b. Undertaking a financial analysis to explore the impact of development cost charges, amenity cost charges, and inclusionary housing programs that create clear and consistent expectations for developers.
  - c. Developing an incentive program for housing development to help reinvigorate a local housing development industry capable of meeting the housing demand into the future. This could include zoning and heritage incentives and relaxations, some relief from development fees and charges, tax relief through a downtown revitalization bylaw, and City-driven/financed projects and partnerships to catalyze new development and establish the quality of development desired by example.



THE ATLIN TERMINAL AND COW BAY MARINA ARE BOTH LOCATED WITHIN THE MARINA DISTRICT.

## 4.2 CITY CORE AND COMMERCIAL LANDS

Commercial (e.g., retail, hotel, services, and office) uses in Prince Rupert are principally located in the City Core neighbourhoods in the Downtown, Marina, and Midtown Districts (see Maps 1A, 1B, and 1C). These areas contain the majority of commercial uses, mixed with park, residential, cultural, transportation, institutional, and tourist service land uses.

The plan accommodates 400,000 square feet of retail and 200,000 square feet of large-format/service retail in the Downtown area, with 100,000 square feet of retail in two neighbourhood retail nodes outside of the Downtown.

### DOWNTOWN DISTRICT

The Downtown District is distinguished by two sub-neighbourhoods, the Upper Town and the Lower Town. The Upper Town hosts shopping streets, offices, terminal buildings, theatres, the Courthouse, hotels, supermarkets, social and health services, along with residential and marina uses. The Lower Town is anchored by the Cow Bay waterfront area marinas, along

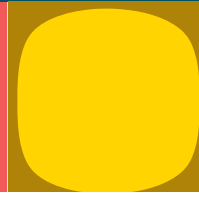
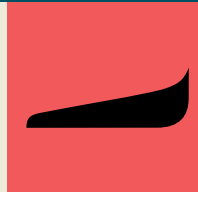
with specialty shopping, restaurants/cafes, neighbourhood licensed establishments, live-work studios, residential, and office uses.

### MIDTOWN DISTRICT

The Midtown District includes some retail uses (e.g., grocery stores, ground floor retail in mixed-use buildings, restaurants/cafes, neighbourhood licensed establishments, shopping centres including mid-sized and large format retail, and educational facilities). This mix of uses in this area includes a significant number of ground-oriented, multi-family buildings, rentals, condos, and social housing.

### MARINA DISTRICT

The Marina District is a newly designated commercial area in Prince Rupert intended to develop with a character complementary to, and reflective of, the Cow Bay neighbourhood. Commercial uses in this comprehensively designed area may include ground floor shops and services, commercial serving recreation and tourism, personal services, supermarkets, public markets, industrial, and mid-sized, large



format retail. The area is also a creative hub for technological enterprises, restaurants/cafes, neighbourhood licensed establishments, studios, galleries, marine manufacturing, education, and live-work uses. As this is also a mixed-use neighbourhood, there are residential uses in the form of condominiums, townhouses, and floating homes. Adding to the livability of this neighbourhood are recreational services, parks, and open spaces that serve the residents and visitors.

## MICRO-NEIGHBOURHOOD CENTRES

Beyond the City Core neighbourhoods are several neighbourhood grocery/convenience stores throughout the residential areas of Prince Rupert. The City's Complete Community Assessment (2024) identifies micro-neighbourhood centers that could be enhanced to create more availability for daily needs, particularly in areas with a higher housing density. This approach will enhance accessibility, reduce the need for longer travel times, and promote vibrant, self-sufficient micro-communities within Prince Rupert.

## BUILDING HEIGHTS

Map 2 illustrates the framework for building heights in the City Core. Most building heights are to be up to four stories throughout the Core, but eight-story buildings will be considered in the Marina District, six-story buildings in the hotel area on 1st Avenue, as well as at the McBride Avenue gateway into the Downtown District between 4th and 3rd Avenues. The intent is to have higher-rise buildings in the new Downtown, but buildings will be lower-rise and less dense as they approach the existing small-scale residential areas west of the City Core.

## CITY CORE POLICY

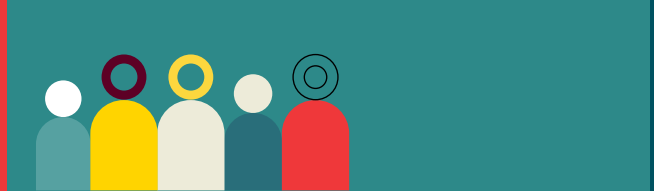
1. The City Core will be a mixed-use area distinguished by three main areas: Downtown, Midtown and the Marina Districts.
2. Explore opportunities to provide access and protect views to the waterfront with all development proposals in the City Core.
3. The City Core building heights will follow a framework identified in Map 2. Building heights will be reduced gradually as they approach small-scale residential neighbourhoods to the west.
4. Improve personal safety and security by incorporating Crime Prevention Through Environmental Design into the City Core.

## COMMERCIAL USE POLICY

1. Support enhancements in the City's existing micro-neighbourhood centers through commercial and institutional development to create more availability for daily needs, particularly in areas with a higher housing density.
2. Enable neighbourhoods outside Downtown to develop additional daily needs by leveraging existing commercial use locations as nodes for development.

## 4.3 INDUSTRIAL

1. Present and proposed industrial uses are designated on Maps 1A and 1B. The growth and vitality of the industrial uses will likely be the main drivers of development in Prince Rupert for decades to come. The City of Prince Rupert is in the process of renewal after decades of economic challenges resulting from the decline of the fishing and forestry industries. The growth in the shipping business through the port



of Prince Rupert has been significant in the past decade and sustained through the local workforce left by the fishing and forestry industries. The Prince Rupert Port Authority anticipates considerable port growth over the next decade. For this to happen positively, renewal of the city, including new residential stock and an attractive, vital City Core, will need to occur. A prosperous future for the City and the Port Authority depends on both working together, such that development is mutually supportive and complementary. Industrial Use Policy

2. This OCP designates port lands as industrial lands on Maps [1A](#) and [1B](#). [Map 1A](#) identifies all industrial (existing and future) lands within the city, including District Lot 444.
3. Recognize the importance of a diverse economy, and seek to enable other industries to develop within the city to create resiliency to negative changes in the port’s activity levels. In addition to creating resiliency, local industrial development can take advantage of the proximity of the port facilities and further add to the local economy.
4. Along with supporting and encouraging industrial development, encourage sustainability practices for new and existing industries. Considerations for new industrial development proposals may include:
  - a. Incorporating one industry’s by-product as feedstock for another industry.
  - b. Using technology and infrastructure to minimize energy use and Greenhouse Gas (GHG) emissions and reduce environmental impact.
  - a. Ensuring that the local environment is protected from industrial uses.

5. Pursue and encourage industry and the Port Authority to expand public access to waterfront lands consistent with the City Core policy.
6. Find opportunities for those industrial properties near the City Core to provide waterfront access and protect views to the waterfront.

## 4.4 PARKS, RECREATION, AND TRAILS

### PARKS AND TRAILS

Parks and trails are essential to the health, identity, and livability of Prince Rupert. As the city grows and evolves, its green spaces, trails, and outdoor amenities must also grow. The community’s unique setting offers extraordinary opportunities to connect people with nature, culture, and each other. The City’s updated Vision for parks and trails is guided by the City’s 2023 Parks and Outdoor Recreation Plan – Connect Rupert and the 2024 Prince Rupert and Area Accessibility Plan.

This section of the OCP recognizes that parks and outdoor spaces are not just recreational assets – they are critical infrastructure that supports environmental resilience, social inclusion, cultural expression, and economic vitality. As Prince Rupert anticipates significant population growth, the City must ensure its parks and recreation system is inclusive, accessible, and responsive to the needs of all residents – youth, Elders, families, newcomers, and Indigenous communities alike.

Together, these policies will help realize the community’s aspiration to become “A City Within a Park” – a place where outdoor spaces are abundant, meaningful, beautiful, and deeply connected to the people who live here.



## PARKS POLICIES:

1. Pursue the recommendations outlined in Rupert Plays and the Prince Rupert and Area Accessibility Plan to provide inclusive, affordable, and resilient parks and recreation amenities that enhance community wellbeing and inspire a healthy and active lifestyle.
2. In collaboration with others, provide more access to and along the waterfront by developing vibrant public spaces and uses, including improvements at Harbour Landing Park and a new waterfront park.
3. Upgrade parks and add more parks for population growth to increase access to nature in the city. This includes pursuing the acquisition and establishment of the new parks and open spaces identified in [Map 3](#):
  - a. A new major waterfront park in the Marina District, which may include open space play areas, an event area and stage, gardens and plazas, a seawall, beach area for water access, First Nations structure, interpretive and programming elements, and food/ café uses.
  - b. A new public wharf in the Lower Town area of the Downtown District. This may include cafes and restaurants, marine berthing facilities, a grand stair and funicular connection to Upper Town, and visitor information services.
  - c. A new Harbour Landing Park with green space, water access, ferry landing, and restaurant/public house services.
  - d. A new Midtown Park serving the Midtown District residents.
4. Replace aging equipment and add more amenities to parks so residents have access to a wide range of outdoor recreation amenities including ball

diamonds, basketball courts, community gardens, off-leash dog parks, a golf course, an outdoor performance area, playgrounds, a pump track/bike park, a running track, a skate park, sports fields, and tennis courts.

5. In new developments where a 5% parkland dedication may be required, the City will decide the location and amount of such parkland or whether cash in lieu of dedication is acceptable.

## TRAIL POLICIES

1. Pursue the recommendations outlined in Rupert Plays and the Prince Rupert and Area Accessibility Plan to improve trails and add more trail connections that enhance community wellbeing and inspire a healthy and active lifestyle.
2. Encourage the dedication and construction of a connected trail system through the formalization of existing trails and the establishment of new trails in priority areas as identified in [Map 3](#), including:
  - a. Improving connections to the multi-use path along the waterfront.
  - b. Improving nature trails within the urban area.
  - c. Improving nature trails outside the urban area.
  - d. Future connections to recreation destinations such as Port Edward, Oliver Lake, and others. Future major projects on Kaien Island will be reviewed in terms of their ability to accommodate part of such a trail network.
3. The approving officer must consider trail dedication as part of highway requirements for future subdivisions.
4. Open and develop historic subdivided and dedicated unconstructed road-ends to the sea as public trails.



THE TRAIL NEXT TO THE PRINCE RUPERT FISH HATCHERY IS AMONG MANY IN OUR AREA.

## RECREATION AND PHYSICAL LITERACY

Recreation supports community capacity building by promoting and improving mental and physical health and strengthening social connections. The role of recreation has evolved from solely providing sport, culture, and leisure facilities and programs to becoming an active partner in public initiatives that foster healthier, more inclusive communities. Canada's aging population presents both challenges and opportunities for the recreation sector to play a meaningful role in addressing societal needs. Keeping older adults active and engaged enhances physical, mental, and social health—reducing hospitalization rates and supporting the ability to age in place. This, in turn, can ease the strain on the local health care system.

### Recreation and Physical Literacy Policies:

5. Encourage trail development and improvements that allow access to nature and viewpoints for people of all abilities (including those in wheelchairs, strollers and walkers), and by a variety of users, as well as interconnections between different community areas.
6. Encourage trail improvements, including trail signage, adequate parking, benches, and related maintenance, to increase the level of satisfaction with the trail experience and to build on this as a defining feature of an active city.
7. Put in place a process for review and approval of all trail works by the City prior to the construction of new trails by public groups.
8. Secure all required land tenures prior to the construction of new trails.

1. Develop and implement a Recreation Plan for the city.
2. Regularly review fees and processes for recreation services in the city.
3. Address barriers in outdoor recreational spaces to promote inclusive access to recreational and preventative health opportunities.

## 4.5 INSTITUTIONAL FACILITIES

Schools and community facilities may be permitted within any of the Long Range Land Use map designations on Maps [1A](#) and [1B](#). Generally, all other institutional facilities are encouraged to locate in the City Core.

The primary jurisdiction for providing schools in Prince Rupert lies with the Province, School District No. 52 (SD52). In addition to the



schools provided by SD52, the Catholic Church provides a kindergarten to Grade 8 facility in Annunciation School. There is also a post-secondary school in Prince Rupert that offers full-time programming, including university courses and associate degrees, diplomas, certificates, a variety of flexible career and college preparation courses, and short-term continuing studies, along with online and workforce-ready certificates.

The City has the following policies concerning the location and management of schools and public recreation facilities:

## INSTITUTIONAL POLICIES

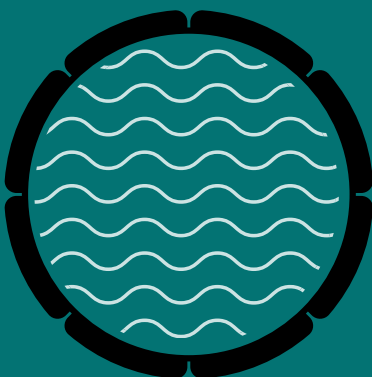
1. Maintain and regularly update the Asset Management Strategy and Roadmap to ensure the management of its institutional facilities is sustainable, data-informed, and aligned with long-term growth, fiscal planning, and community service levels.
2. Each elementary school should serve a local residential neighbourhood and should therefore be included in developing new greenfield neighbourhoods.
3. Elementary schools should not be developed adjacent to arterial roads.
4. School facilities should be designed and managed to maximize joint public use of recreational activities.
5. Municipal parks may be acquired and located adjacent to schools where mutually beneficial.
6. The provision of off-site road, sidewalk, boulevard, transit bay, and utilities required to service a school use should be secured prior to rezoning or developing lands for school use.
7. Sufficient school on-site vehicle passenger loading and unloading facilities will be required.
8. As the need for improvements and/or replacement of civic facilities becomes apparent and as resources allow, the City may upgrade existing facilities and/or provide new ones. Buildings that may require upgrading or replacement in the future include City Hall and the Recreation Complex.
9. Encourage collaborations and partnerships with the School District with a view to maximizing public benefit by:
  - a. Consulting on recreation programs that support School District learning objectives.
  - b. Developing joint use agreements for school and City property and facilities when opportunities arise (e.g., after-hour community use of School District properties or School use of the City's recreational facilities).
  - c. Finding community uses for potential surplus properties.
  - d. Future schools are permitted in any of the land use designations in [Map 1A](#). However, elementary schools are encouraged to locate within new residential neighbourhoods.
10. Support new micro-neighbourhood centres in existing residential areas through programming of community facilities, schools, childcare and other public buildings through partnerships with community organizations to enhance social services, recreation, education, and food security.

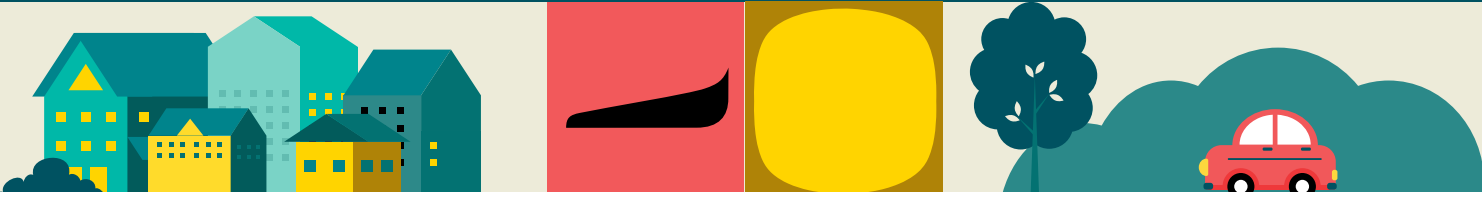


# COMMUNITY DEVELOPMENT POLICIES

In addition to the required components of an Official Community Plan, the *Local Government Act* enables municipalities to include policies on a broader range of community priorities such as environmental management, cultural and heritage resources, transportation, infrastructure, and more.

These topics are often addressed through separate, more detailed plans. This OCP provides overarching direction and a unifying vision for these community priorities, while the individual plans will guide implementation and action on specific issues.





## 5.1 CLIMATE CHANGE AND GHG EMISSIONS

Prince Rupert is already experiencing the impacts of climate change, including more frequent and intense storms, increased rainfall, changing ecosystems, and risks from sea level rise, flooding, and landslides. As a coastal and remote community, Prince Rupert also faces unique vulnerabilities and service delivery challenges during extreme weather events.

The City's Climate Change Adaptation Plan (2025) outlines the risks, vulnerabilities, and priority actions needed to prepare for and respond to these impacts. This section of the Official Community Plan (OCP) establishes the land use and infrastructure policy direction that integrates climate resilience into future development and municipal decision-making.

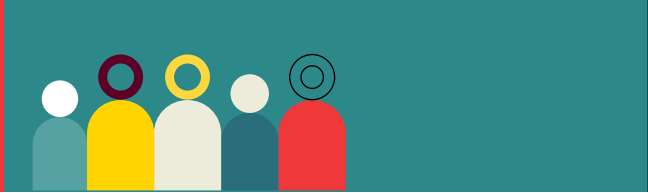
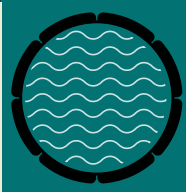
By embedding adaptation into the OCP, the City aims to reduce risk to people, property, and critical infrastructure; ensure continuity of essential services; and support a safe, healthy, and sustainable community under changing climate conditions.

The City has a number of tools it can consider for reducing community Greenhouse Gas (GHG) emissions including Development Permit Areas (DPAs), Roads and Subdivision Bylaw, Civic Buildings, Fleet Management, Transit Oriented Development, and Natural Carbon Sequestration, the Energy Step Code, building retrofit programs including Property Assessed Clean Energy, neutralization of GHG emissions from demolitions, traffic and transportation management, electrification of buildings and vehicles, use and encouragement of alternative clean energy and promotion of cycling and walking in relation to all decision-making.

### CLIMATE CHANGE AND GHG EMISSIONS POLICIES

The following policies will guide the City of Prince Rupert in its actions to mitigate and adapt to Climate Change:

1. Use and regularly review the Climate Change Adaptation Plan as the guiding document for identifying and responding to climate risks in land use, infrastructure, and community service planning.
2. Use and regularly review the Community Energy and Emissions Plan as the guiding document for the City's mitigation planning work.
3. Ensure that corporate directions, corresponding plans, infrastructure development, and capital projects consider climate change adaptation and mitigation measures.
4. Aspire to meet or exceed an 80% GHG reduction of 2007 levels by 2050.
5. Ensure residents have access to a resilient transportation infrastructure and emergency public transit.
6. Reduce the likelihood of negative human and wildlife interactions by supporting a resilient tree canopy, encouraging invasive species reporting, and raising awareness of Prince Rupert's biodiversity.
7. Undertake long-term planning with a climate lens for water major assets to reduce water system disruptions and potential declines in the fresh water supply.



- 8. Focus on temporary shelter, household and community emergency preparedness and uptake of climate resilience retrofits to support health and wellbeing. DPA guidelines include developing energy efficiency guidelines for small-scale homes, duplexes, and multi-family residential, commercial, industrial, and institutional buildings.
- 9. Address potential disruptions in access to essential services by improving household adaptive capacity, exploring partnerships to improve trust with vulnerable populations, bolstering emergency support services, and improving energy and telecommunications during emergency response.
- 10. Support improvements to local food infrastructure to address potential declines in the local food supply.
- 11. Support the electrification of vehicles and the use and encouragement of alternative clean energy.
- 12. Support building retrofitting programs, including Property Assessed Clean Energy and neutralizing GHG emissions from demolitions.

## 5.2 ENVIRONMENTALLY SENSITIVE LANDS RESTRICTIONS

In reviewing future development proposals, the City may require the protection of sensitive natural areas and will also encourage the retention of significant naturally vegetated areas between development nodes. These natural areas may include provision for trail linkages and interpretive sites.

The City's development footprint for residential and commercial uses has largely been contained within the original Brett and Hall plan. The city's challenging topography creates natural protection for its natural spaces, which offer limited development opportunities.

### ENVIRONMENTALLY SENSITIVE LANDS POLICY

- 1. Map 1A identifies parks and open spaces, which include wilderness lands. These are environmentally sensitive lands and are intended to remain ecologically intact.
- 2. Ensure natural assets are managed to recognize, evaluate, and protect ecological infrastructure as essential components of sustainable service delivery.
- 3. Public Access to the wilderness lands will be allowed, but in accordance with good stewardship practices. To this end, the City may develop a Wilderness User Master Strategy in consultation with First Nations. In addition, public utilities, rifle ranges, beekeeping, and roads may also be considered in this area.
- 4. In addition to the protection of the wilderness lands, the City also has the following policy directions regarding environmentally sensitive lands:
  - a. New development on greenfield sites in the City will be required to follow relevant provincial regulations pertaining to environmental protection, ecological function and riparian areas.



- b.** Streams and their riparian areas will be protected. Any development that infringes on these areas will be done in accordance with the Riparian Areas Regulation and under the guidance of a qualified professional biologist. The City has a Development Permit Area designation and process to achieve this policy.
- c.** Slopes in excess of 30% should retain their vegetation, and where infringement on these areas is necessary, a geotechnical assessment and measures must be prepared and followed.
- d.** Intertidal marsh areas are critical marine life habitats, providing both refuge and spawning habitat, and therefore will be protected from development impacts.
- e.** The quality of local marine waters is important to the health of marine life and those who live off it. Therefore, the City may undertake actions to ensure these waters are clean. Such actions may include:
  - » Working with the Prince Rupert Port Authority to ensure its operations protect local marine water quality.
  - » Improving the quality of storm and sewer water discharge into the Marine water through treatment and source control measures.

## 5.3 FOOD SYSTEMS

Food security is an increasingly important issue for municipal governments. Prince Rupert's Official Community Plan includes a dedicated section on Food Systems to ensure that future development supports the subsistence needs of future generations. While the city lacks a tradition of land-based agriculture due to limited arable land, local food has long been central to the region's identity. First Nations communities continue to harvest over a hundred marine and terrestrial foods, and the surrounding waters have historically sustained both the Tsimshian people and inland Nations. Though the commercial fishing industry has declined, initiatives like the Coastal Shellfish scallop farm demonstrate the potential of combining traditional knowledge with modern aquaculture.

Despite the absence of large-scale agriculture, informal food production – such as greenhouses, small orchards, and shared harvests – remains active in Prince Rupert. Interest in local food systems is growing, driven by concerns about sustainability, climate change, and public health. Urban agriculture and marine-based food initiatives offer promising opportunities to enhance food security, improve community health, and support local economies. This section of the OCP outlines how Prince Rupert can support these efforts through resource provision, community programs, advocacy, and policy development tailored to the city's unique environmental and cultural context.



## FOOD SYSTEMS POLICY

1. Implement and regularly review the City's Local Food System Strategy to guide the City's role and work in the local food system.
2. Recognize the value of food production in the history of the people inhabiting the area and the potential of local food production in the future of the city.
3. Support and participate in local food initiatives, actors, and action plans as capacity and resources permit, recognizing the City's role in advancing community food security and resilience.
4. Support urban agriculture. The City's residential and commercial land designations include urban agriculture as a permitted use. These will be implemented by zoning amendments following the preparation of a community food strategy.
5. May undertake actions to encourage local food entrepreneurs to grow, process, and sell food.
6. May encourage the development of a local food market in the Downtown District to enable local small-scale producers and new industry to showcase local food.
7. Support community and/or demonstration gardens in neighbourhoods and downtown locations.
8. May facilitate knowledge and skill development for growing and preserving food in recreation center programming and on the City's website.
9. May create new community-wide celebrations around food.
10. Encourage the establishment of local food processing and distribution.

11. May develop urban agriculture guidelines and regulations to ensure farming in urban areas is compatible with surrounding land uses.
12. May amend the Zoning Bylaw and Business Bylaw to permit urban agriculture and sales as appropriate. Regulations should ensure that urban agriculture uses do not create rodent or odour problems in neighbourhoods.

## 5.4 HAZARDOUS LANDS

This section outlines policies for hazardous lands not currently addressed in the City's Hazardous Areas Development Permit Area. A comprehensive review of all development permit areas will be undertaken in the future to ensure they reflect current and emerging hazard considerations.

### STEEP SLOPES

Steep slopes are currently covered under the existing Hazardous Areas Development Permit Area.

### SEA LEVEL RISE

In January 2018, the Province of British Columbia adopted an amendment to the Flood Hazard Area Land Use Management Guidelines by incorporating sea level rise into building standards in all coastal areas to include storm surge, wave effect, and freeboard when determining the Flood Construction Level (FCL) of new building construction.

Prince Rupert's Climate Change Adaptation Plan indicates that the rate of sea level rise is expected to increase in Prince Rupert in the future. However, projections of the rise in sea level are highly uncertain. The Sea Level Rise



Policy for British Columbia (BC Ministry of Environment and Parks, 2011b) recommends using a 1.0 metre rise in global mean sea level between the year 2000 and 2100 for planning purposes.

The Climate Change Adaptation Plan indicates that the City should consider a local sea level rise analysis. This could include detailed local analysis, including general sea level rise predictions, local coastal conditions, and anticipated wave effects. The result of this analysis can serve to provide informed decisions for construction projects within anticipated inundation areas beyond the high-level guidance from the BC Ministry of Environment.

### Sea Level Rise Policy

1. Undertake local sea level rise analysis and mapping to enable the development of projects along the waterfront that will be protected from anticipated sea-water flooding.
2. Review the Hazard Areas Development Permit Area to ensure that policies reflect appropriate consideration of rising sea levels.

### TSUNAMI RISK

In 2019, the City of Prince Rupert had a Tsunami Risk Assessment prepared, which found no current residential or commercial buildings, schools, emergency services, or critical infrastructure at risk from seismically generated Tsunami hazards. It was found that there was some risk for port facilities and marine infrastructure. It also found some risk resulting from a landslide-generated tsunami. While there are no observed slopes in the area that might be unstable, it was recommended

to conduct a regional slope assessment to determine any slopes that might pose a risk. It was also recommended that low areas considered for waterfront development be assessed for tsunami risk and that houseboats not be permitted in high wave velocity areas like Fern Passage.

### Tsunami Risk Policy

1. Consider undertaking a regional slope assessment to determine any unstable slopes that might pose a tsunami risk.
2. Any low areas that might be considered for waterfront development will be assessed for tsunami risk at the owner's cost.
3. Houseboats will not be permitted in high wave velocity areas like Fern Passage.
4. Review the Hazard Areas Development Permit Area to ensure policies reflect appropriate consideration of tsunami risk.

### INTERFACE FIRE HAZARD

While interface fire is a significant risk in most places in British Columbia, Prince Rupert's year-round climate conditions reduce wildfire risk; however, the City's Climate Change Adaptation Plan does highlight an increased risk of wildfire over time. Although the climate indicators show an increasing threat of a wildfire, the natural environment is on a natural fire break and less likely to change in a future climate.

### Interface Fire Hazard Policy

1. Assess wildfire risk with the fire department and BC Wildfire Service every five years, to determine if changes are needed to the City's current wildfire approach.
2. Review the Hazard Areas Development Permit Area to ensure policies reflect appropriate interface fire risk consideration.



## 5.5 HERITAGE RESOURCES

The City has designated three properties as heritage sites. These are the First Presbyterian Church, the Pillsbury House, and the Provincial Courthouse. An updated assessment will be made of historic and heritage landmarks both inside and outside the City's Core to allow for better interpretation of heritage buildings and access to features of historic value, including some of the Second World War fortifications above the harbour area.

### HERITAGE POLICY

1. Stewardship of historic landmarks, including but not limited to the “thousand steps,” totem poles, and petroglyphs. This includes ongoing maintenance, protection, and recognition of their cultural and historical significance.
2. Engage residents and First Nations in meaningful dialogue to guide the preservation and interpretation of heritage features. This engagement will ensure that diverse perspectives and traditional knowledge inform decision-making.
3. Through collaborative processes, identify and prioritize heritage features for preservation based on cultural, historical, and community values.
4. Where appropriate, support public access to heritage features and develop interpretive materials or experiences that foster education and appreciation of these sites.



**IN THE ENTRANCE TO DOWNTOWN, THE PROVINCIAL COURTHOUSE IS ONE OF PRINCE RUPERT'S MOST PROMINENT HERITAGE BUILDINGS.**

## 5.6 MAJOR PROJECTS

Prince Rupert is promised a bright future, with the expected port development to double over the next ten years. Given past interest in other major projects in Prince Rupert, it is prudent to have a policy that would allow the City to respond to the demands of such a project materializing. This section benefits from the recent work done by Prince Rupert planning staff and Council, who have developed a policy to guide major projects. Major projects are defined as:

- a. New industrial projects with a capital expenditure in excess of \$250 million.
- b. Any multi-year project requiring an outside workforce and resulting in a shadow population of transient workers.

Major project review will be evaluated in terms of the following objectives and policies.



## URBAN CONTAINMENT OBJECTIVES

The original Townsite Plan (Figure 1) development footprint shall be an urban containment boundary to ensure that all new residential and commercial development in Prince Rupert falls within the urban footprint outlined in the 1907 Townsite Plan. New residential and commercial development will not be permitted outside the proposed 1907 footprint area. New development applications in support of a major project will be prioritized as follows:

- » Priority #1: Infill Development Areas
- » Priority #2: Future Neighbourhoods

These development areas are located within the Urban Containment Boundary and identified in [Map 8](#).

## MAJOR PROJECT POLICIES

Staff and Council originally drafted a portion of this policy suite during the Planning for Major Projects initiative. Policies were developed to address needs identified in a community-wide survey on housing and other community priorities, population projections if one or more major projects were to come to Prince Rupert, an evaluation of local housing densities, conditions, and vacancies, as well as other relevant data sets.

1. Only industrial, transportation, utility, roads, and parkland uses will be permitted outside of the Urban Containment Boundary.
2. Council will consider temporary and fully serviced camps for a major project on a case-by-case basis.
3. The City of Prince Rupert will allow the extension of municipal services and roads to Future Neighbourhoods Areas at any time as long as the Development Proponent pays all associated costs.
4. All new multi-family residential and mixed-use developments on sites developed to serve major projects larger than 0.4 ha (1 acre) should include the key elements of complete communities and a mixture of densities and building forms over the entire development site.
5. All new multi-family residential and mixed-use developments on sites developed to serve major projects should provide community amenities for occupants in accordance with the City Community Amenity Policy and be located within close proximity, no more than 1 kilometre or a 20-minute walk from existing parks and recreation facilities.
6. Major new project proposals should prepare a Fire Protection plan in consultation with the City's Fire Department to ensure that people and property within and adjacent to major project sites are adequately protected from fire risk.
7. The City of Prince Rupert should develop and implement Amenity Cost Charges for major projects to ensure that such projects contribute tangibly to services and amenities that significantly contribute to the quality of life for Prince Rupert residents. This will include, among other possible amenities, a contribution of the amenities or cash in lieu, having a value of \$2,000 per bed in work camps for use in affordable housing projects.



8. Council shall consider the timeframe associated with any major project. For example, if a major project will have a temporary need for housing (e.g., less than 10 years) then subject to the BC Building Code, temporary housing forms will be considered provided that:
  - a. Sufficient bonding is provided to ensure the removal of the temporary housing supply.
  - b. All necessary infrastructure services (water, sewer, roads, etc.) meet current municipal construction standards.

However, if the need for housing the workers associated with major projects is a long-term need (greater than 10 years), such housing and residential development shall meet the same standards and quality as any other new residential development, complete with neighbourhood services such as parks, trails, bike paths, recreation, etc.

## 5.7 SOCIAL AND CULTURAL DEVELOPMENT

The strength and vitality of a community depend on a healthy environment and a strong economy that benefits everyone with services, employment opportunities, and recreational services that help with residents' physical health. It is also dependent on the community's diverse social and cultural needs being addressed. This includes social services to help residents, cultural services to enrich residents' lives, social events, celebrations, and festivals to develop and maintain positive community experiences and satisfaction. A community

with a high degree of social satisfaction attracts and keeps newcomers.

Social and cultural services are not frivolous efforts. The social and economic well-being of a community are mutually interdependent. Business growth in the city requires new workers to be recruited to live in Prince Rupert; new workers and their families will assess the City's social services to determine if they meet their needs. Currently, the City has important cultural facilities in the Downtown District, such as the Kwintsa Railway Museum, Museum of Northern British Columbia, and Port Interpretive Centre.

This section of the Official Community Plan builds on the City's 2024 Social Development Assessment, 2022 Reconciliation Policy Framework, and 2020 Child Care Assessment and Action Plan, which provide a comprehensive understanding of the community's strengths, gaps, and opportunities.

The City's role in social development is not to replace the responsibilities of senior governments, but to act as a convener, advocate, and enabler by supporting local organizations, facilitating partnerships, and embedding considerations of social development and childcare into the City's policies and practices.

### SOCIAL AND CULTURAL POLICIES

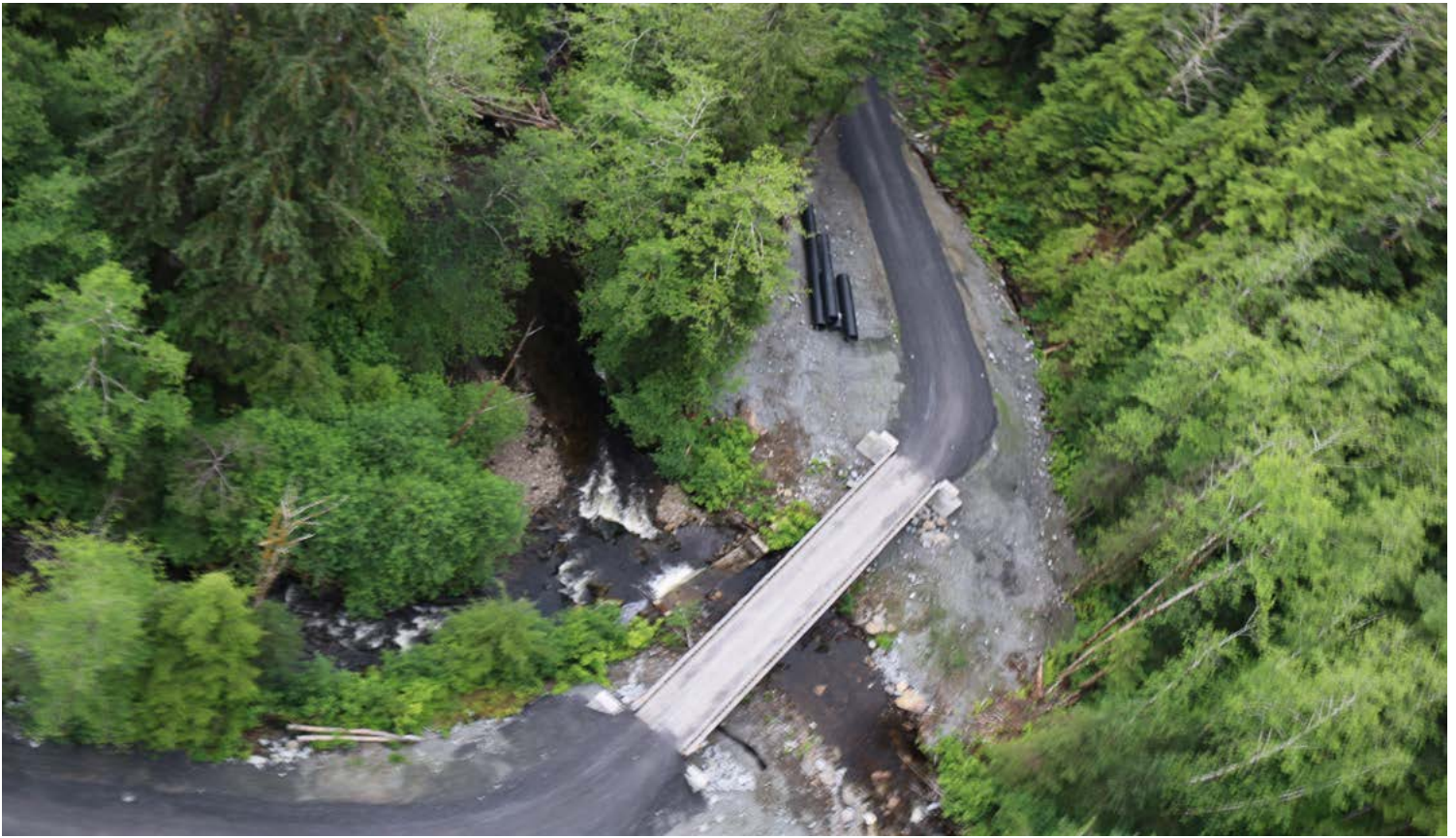
1. Align social and cultural planning, policy development, and implementation with the guiding principles, findings, and recommendations of the 2024 Social Development Assessment, 2022 Reconciliation Policy Framework, and 2020 Child Care Assessment and Action Plan.



2. Develop and maintain a Social Development Strategy that includes measurable goals, timelines, and indicators. This strategy will be reviewed and updated every five years, and progress will be reported publicly to ensure transparency and accountability.
3. The City of Prince Rupert has adopted the United Nations Declaration on the Rights of Indigenous Peoples (UNDRIP) as the framework for reconciliation and commits to developing an implementation plan in partnership with Indigenous governments, organizations, and individuals.
4. Given the historic presence of First Nations and their ongoing use of the lands in their territory, pursue opportunities to recognize the territory and culture of First Nations in the Downtown District and on the waterfront.
5. Continue to support and participate in the Indigenous Relations Committee and engage Indigenous Nations and service providers in all aspects of social planning, including housing, health, and cultural initiatives.
6. Encourage a very high maintenance of local cultural facilities and explore opportunities to integrate cultural programming into public spaces, including parks, civic buildings, and community events.
7. Support regular convening of social service providers to foster collaboration, reduce duplication, and identify emerging needs. This includes maintaining and updating a community service inventory.
8. Advocate for senior levels of government to sustain investment in housing, mental health, addiction services, and homelessness prevention, and support local organizations in accessing funding and resources.
9. Support the development of new childcare spaces by:
  - a. Supporting community childcare programs and funding applications.
  - b. Supporting childcare spaces in all commercial and public zones.
  - c. Introducing revitalization tax exemption bylaws to create incentives for developing new childcare spaces and continue considering permissive tax exemptions.
  - d. Supporting community partners in developing additional childcare spaces in the community using City property if available.
  - e. Considering partnerships and advocacy for childcare services.
  - f. Working with senior levels of governments and service organizations, as many childcare issues are beyond the sole influence and scope of the City.

## 5.8 SEWER, WATER, AND WASTE INFRASTRUCTURE

The Local Government Act requires the OCP to describe and identify the approximate location and phasing of any major road, sewer, water, and waste disposal systems. The City's infrastructure was built to service 25,000 people, but it is old, constructed in some cases with substandard materials, and has incurred significant deferred maintenance. With much of the network at the end of its useful service life, and the escalating costs of replacement, the infrastructure gap is large.



AN OVERHEAD VIEW OF THE ROAD UP TO THE DAM AT WOODWORTH LAKE, DEVELOPED IN 2017 - 2018.

## THE BIG PROJECT

The Big Infrastructure Gap (BIG) project is the City of Prince Rupert's plan to upgrade and replace the most critical water and sewer infrastructure. Through the Infrastructure Replacement Strategy, the City identified 26 kilometres of water mains that are a combination of high-risk-of-failure and high-likelihood-of-failure infrastructure. This is a significant generational investment into the City's infrastructure that will help secure it for years to come. It will improve the reliability of service delivery, enhance water quality, and minimize future disruptions in the distribution system.

## WATER AND SEWER SYSTEM

### Water System

The City's water distribution system can deliver up to 10 million gallons of potable water daily, serving commercial needs and a population of about 25,000. Water is sourced from the Woodworth Lake Reservoir and flows by gravity to a chlorination station near Shawatlan Lake. From there, it travels through submarine crossings and pump stations before reaching homes and businesses. Much of the infrastructure was installed between 1950 and 1980, meaning a large portion is now reaching the end of its useful life. Over half of the system is more than 50 years old, and about 14 percent is over 90 years old.



Many older pipes were installed before the 1960s using materials and construction methods now considered substandard. The City has already addressed some of these issues, including replacing the 110-year-old Woodworth Lake Dam in 2022 and upgrading 2.2 kilometres of cast iron pipe with modern materials. Despite these improvements, critical components such as one of the submarine crossings at Fern Point remain in poor condition and require replacement. A water treatment feasibility study completed in 2018 led to a grant for a new treatment facility, which is currently in the engineering phase.

While replacing this infrastructure is costly, continuing to repair frequent breaks is even more expensive. Investing in proactive infrastructure renewal is more cost-effective than repeatedly repairing failing systems. The City has faced decades of limited budgets due to reduced tax revenues, but staff and Council are now working to secure new funding sources to prevent service disruptions and avoid the risk of catastrophic failure.

### **Sewage System**

The City's sewage system includes collection lines, trunk sewers, and outfalls that discharge into the ocean. Wastewater is collected from properties and flows through a network of increasingly larger pipes. The system dates back to the early 1900s and includes ten major catchment areas that release into Prince Rupert Harbour. Ideally, stormwater and sanitary sewage should be separated, where feasible, to prevent flooding during heavy rain and to allow for future treatment.

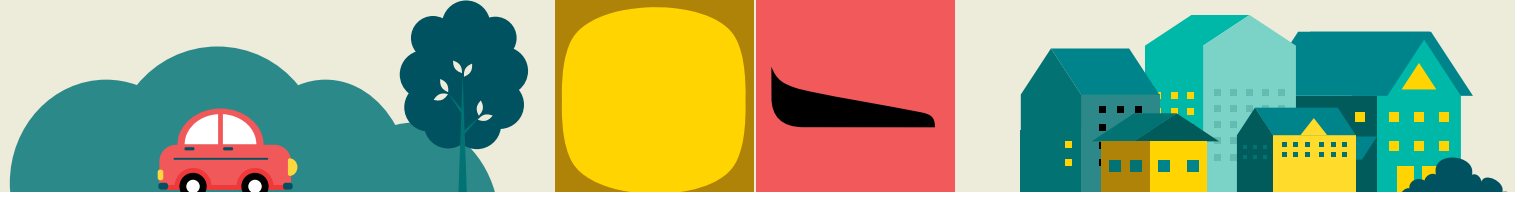
Sewer pipes typically last 60 to 80 years if installed properly. The City has about 90 kilometres of sanitary sewer, which would require replacing 1.1 kilometres yearly to stay on track. Over the past 20 years, only 6 kilometres

have been replaced. It is estimated that 40 kilometres of the system are now beyond their expected lifespan, while 50 kilometres remain within their expected lifespan. The age and condition of the pipes vary, and installation records are incomplete, making it difficult to plan upgrades effectively.

The City is actively responding to a Federal mandate for wastewater treatment by 2030, reducing the number of outfalls in the community, and working to twin storm/sewer lines in failing areas as pre-cursor works. For treatment, the City is beginning to implement an environmentally friendly and cost effective wastewater treatment method using a wetland treatment system along Park Avenue to treat sewage from approximately 100 homes, with the intent to expand that technology throughout the community once it is confirmed successful. This system uses natural processes involving soil, plants, and microbes to treat wastewater, and is an alternative to mechanical/chemical treatment methods that are more energy and staffing intensive.

### **WASTE DISPOSAL**

The City owns and operates a landfill, liquid waste disposal site, and recycling facility on Ridley Island Road. Prince Rupert's landfill currently meets capacity thanks to a recent expansion to the municipal landfill, however with community renewal, development, and removal of dilapidated buildings comes additional strain on space. To mitigate future capacity issues, the City may seek to promote principles like the circular economy and zero-waste practices. Considering these principles, the City may explore opportunities to utilize waste-to-energy and recycling programs allowable under the Environment Management Act, building on the recycling program already in place. Such programs may include, but are



not limited to, composting facilities, organics diversion, or solid waste combustion facilities. These programs aim to minimize the solid waste input into landfill facilities while utilizing the energy potential these refuse contain. The City should ensure that any option pursued for waste reduction will align with the City's goal of Greenhouse Gas (GHG) Emissions Reductions.

### Waste Disposal Policy

1. Close the current landfill cell and open a new cell to service the City for the next 50 years.
2. Continue to support the City's recycling program, and consider additional options towards an overall aim of waste diversion .

### LANDFILL

Prince Rupert's landfill capacity is sufficient for the next several years, however with community renewal, development, and removal of dilapidated buildings comes additional strain on space and there is anticipated need to increase capacity within the next decade. To mitigate future capacity issues, the City may seek to promote principles like the circular economy and zero-waste practices. Considering these principles, the City may explore opportunities to utilize waste-to-energy and recycling programs allowable under the Environment Management Act, building on the recycling program already in place. Such programs may include, but are not limited to, composting facilities, organics diversion, or solid waste combustion facilities. These programs aim to minimize the solid waste input into landfill facilities while utilizing the energy potential these refuse contain. The City should ensure that any option pursued for waste reduction will align with the City's goal of Greenhouse Gas (GHG) Emissions Reductions.

### Landfill Reduction Policy

1. Aim to increase public awareness and action in Reducing, Reusing, and Recycling (the Three R's) by:
  - a. Improving access to recycling and re-use opportunities.
  - b. Supporting public education on the benefits of the Three R's.
  - c. Promoting the purchasing habits that prioritize the least GHG-intensive products and services.
2. Aim to reduce the rate of solid waste disposal into the landfill by:
  - a. Continuing to monitor the per capita rate of waste disposal and recycling as the primary means of measuring achievement of such waste reduction objectives.
  - b. Exploring waste-to-energy infrastructure with consideration of the proposed GHG reduction goals to reduce the physical footprint of solid waste while using the energy resources the waste contains.
  - c. Pursuing ways to allow conversion, waste burning and demolition/ disposal sites.
3. Aim to improve the current landfill by:
  - a. Encouraging the use of new technology to minimize odours and other pollutants.
  - b. Pursuing ways to allow for the rehabilitation of the existing landfill.



A LOCAL QUARRY THAT SERVICES PRINCE RUPERT

## 5.9 SAND AND GRAVEL EXTRACTION

Soil and Gravel aggregates are an essential resource for the development and maintenance of the City. They are required for road construction, housing, and commercial and industrial development. Prince Rupert has a particular need for good fill in new development, given the presence of, often deep, muskeg within the city limits. As sand is not available within the city boundaries it has to be imported into the community. [Map 4](#) identifies locations where gravel is sourced. When gravel extraction from a site is complete, the City should ensure that it is either returned to an acceptable natural, vegetated condition without any unstable slopes or open pits, or it should be repurposed and prepared for other development, if that is consistent with [Maps 1A and 1B](#).

### SAND AND GRAVEL POLICY

1. Regularly evaluate policies that manage sand and gravel in Prince Rupert.
2. When the sand and gravel extraction from a site is complete, take actions to see that the site is either returned to an

acceptable natural, vegetated condition without any unstable slopes or open pits, or is repurposed and prepared for other development, if that is consistent with the land use [Map 1A](#) and [1B](#), which are located in the Appendix.

3. Explore District Lot 444 for the presence of usable sand and gravel resources.
4. Discourage aggregate resource extraction that creates undue impact on neighbourhood residential uses or excessive truck traffic, safety, and road condition issues.

## 5.10 TRANSPORTATION

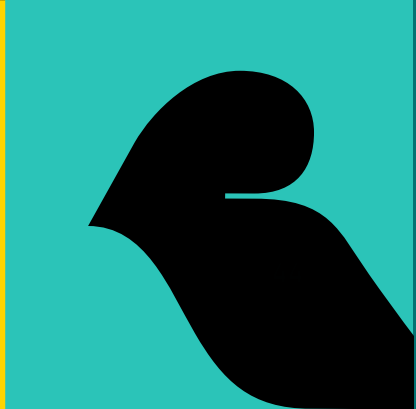
The approach to transportation planning has increasingly focused on the interconnectivity of different modes (e.g., walking, transit, cycling) and on achieving a shift of modes from automobiles to other forms. This approach coordinates and rationalizes the connections between modes and maximizes the efficiencies and effectiveness of the different modes.



Connect Rupert, the City's Transportation Plan, establishes a strategic vision and values that will guide the City's transportation and land-use decisions, public investments over the next 5- to 10 years, and the long term. The plan guides the City towards its vision of being a world-class port city that is vibrant, resilient, and sustainable.

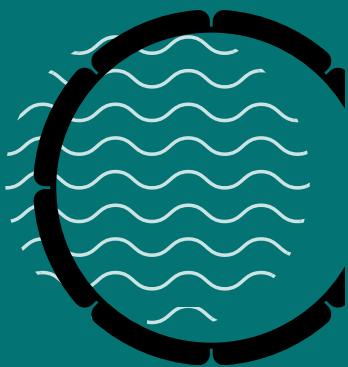
## TRANSPORTATION POLICY

1. Pursue the commitments made in Connect Rupert to create a transportation network that serves community members of all ages and abilities through improvements to modal interconnectivity, active transportation, public transportation, and driving infrastructure.
2. Enhance modal interconnectivity by improving the quality of connections within mobility hubs (as identified in [Map 6C](#)), improving the accessibility of travel information, and improving access to and from station, terminal areas, and Prince Rupert.
3. Enhance the city's active transportation network by developing complete street standards, building a connected and accessible sidewalk and cycling network (as shown in [Maps 6D](#) and [6E](#)), and designing universally accessible streets, trails, and crossings.
4. Focus on quick-build, year-round infrastructure and support programs that encourage walking, cycling, and rolling for people of all ages and abilities.
5. Improve public transportation by working with BC Transit to enhance service reliability, connectivity, and accessibility while improving the overall customer experience.
6. Support a broader range of transportation options by managing curbside use, aligning taxi supply with demand, encouraging ride-hailing, and partnering to improve school travel for children and youth.
7. Make the road network safer for all users by designing people-first streets, maintaining infrastructure, and improving safety and accessibility for all modes of travel.
8. Support future-ready mobility by accommodating goods movement, electrification, automation, and shared transportation, while managing parking to meet the needs of residents, visitors, and businesses.



# DEVELOPMENT PERMIT AREAS

Major city areas are divided into Development Permit Areas, with corresponding guidelines to guide the development of each area.





## 6.1 DEVELOPMENT PERMIT AREA DESIGNATION

The Local Government Act allows municipalities to designate Development Permit Areas (DPA) within their OCPs. These DPAs can be established for several reasons, including protecting natural areas or discouraging development in hazardous lands. Unless exempted under Section 6.9, the Local Government Act stipulates that no subdivision of land or alteration of land (including removing vegetation, construction of, addition to, or alteration of a building or other structure) is to occur unless the applicant first obtains a Development Permit (DP).

A Development Permit issued must follow the Development Permit Area (DPA) guidelines, which identify what is permissible in each DPA. Where a guideline is not appropriate to the particular circumstances, the City of Prince Rupert may deem the guideline not applicable. The Guidelines are provided in Appendices A to F. Sections 6.2 to 6.7 outline each DPA's designation, justification, and objectives.

The City of Prince Rupert OCP designates DPAs for the protection of the natural environment, the protection of development from hazardous conditions, and the establishment of objectives for the form and character of commercial, industrial or multi-family residential development. In particular, the following areas are designated DPAs:

1. City Core (includes multifamily, industrial and commercial lands)
  - 1.1 Marina District
  - 1.2 Downtown District
  - 1.3 Midtown District
2. General Multifamily Areas
3. General Commercial Areas

4. Industrial Areas
5. Environmentally Sensitive (Riparian) Areas
6. Hazardous Areas

The issuance of a DP is the formal approval from the City for the built form, character, and layout of a proposed new development or redevelopment. The DP addresses the siting, massing, form, and character of buildings, landscape plans, parking layout and requirements, environmental protection in environmentally sensitive areas, and protection of development from hazardous conditions. Development Permits are required before Building Permits can be issued or before subdivisions are approved.

In accordance with the Local Government Act, the OCP must:

- a. Describe the special conditions or objectives that justify the designation.
- b. Specify guidelines respecting the manner by which the special conditions or objectives will be addressed.
- c. Specify conditions under which a Development Permit under section 489 would not be required. The Development Permit Area sections listed below list specific exemptions for developments from DP processes.

The Local Government Act also allows the City to include some variances (except for land use or density variances) in a Development Permit, provided that the variances enable the proposed development to comply with the adopted DP guidelines or help achieve an OCP policy or objective.



## 6.2 DEVELOPMENT PERMIT EXEMPTIONS

The following are exempt from the form and character development permit application approval process:

1. Interior renovations.
2. Renovations or maintenance to the façade of a building that do not change colour, materials, or façade articulation.
3. Minor renovations less than 55 m<sup>2</sup> (592 square feet) gross floor area are added to the building or constructed as a new detached building, and the changes are either:
  - a. consistent with the existing building, having no substantial changes in materials, colours, or façade articulation; or
  - b. consistent with the development permit guidelines for “Form and Character Considerations”, “Colour”, and applicable “Additional Architectural Guidelines” addressing materials, colour, and façade articulation.
4. Decks or patios not exceeding an area of 20 m<sup>2</sup> (215 square feet).
5. Replacement of a roof with the same or similar style and building materials.
6. Underground service connections.
7. Addition of rooftop equipment within a screened area approved by the City.
8. Replacement of sign faces with no change in the location, size, or type of signage.
9. Addition of canopies or other decorative building features such as mullions (decorative strip between window panel casements), and windows.

10. Utility buildings (e.g., pump stations, electrical and telephone kiosks) not exceeding an area of 200 m<sup>2</sup>, and accessory buildings not exceeding an area of 50 m<sup>2</sup>.
11. Interpretive signage, approved by the City, which provides general or tourist information and is located on City-owned property, other publicly owned property, or any road right-of-way.

For areas designated within the Riparian Development Permit Area, these conditions do not require a development permit:

1. The reconstruction or repair of a permanent structure in accordance with Sections 528 and 529 of the Local Government Act, if the structure remains within its existing boundaries.
2. The construction of accessory buildings outside of the Streamside Protection and Enhancement Area (SPEA) designated by a Development Permit previously issued by Council, in accordance with the terms of that Development Permit.
3. Changes to existing buildings or structures’ exterior cladding and roofs.
4. Removal of invasive vegetation with the consultation of a qualified environmental professional and the immediate replacement with native vegetation.
5. Existing land uses and buildings, provided that the land area is not increased and the existing building is not expanded.
6. If the applicant submits a letter from a professional qualified biologist certifying that the entire proposed development area is outside of the Riparian Development Permit Area.



7. The development and upkeep of works by the City or its authorized person(s) are exempt from the formal development approval process, but the works must comply with the assessments and recommendations of a professional qualified biologist under the Riparian Areas Regulation Assessment methodology and other applicable environmental regulations.

## 6.3 DEVELOPMENT APPROVAL INFORMATION AREA

### PURPOSE

The Local Government Act provides the authority for the City to specify circumstances and designate areas in which development may not proceed until information about potential impacts is provided and assessed by the City. This additional information can be required where the activity involves a rezoning, development permit or temporary use permit. This information may be regarding:

- Transportation patterns, including traffic flow
- Local infrastructure
- Public facilities, including schools and parks
- Community services
- The natural environment of the area affected

### AREA AFFECTED AND CIRCUMSTANCES

The entire area within the City of Prince Rupert is established as a Development Approval Information Area.

### OBJECTIVE

The intent of establishing this Development Approval Information Area is to ensure that potentially negative impacts of proposed developments are identified and documented during the development review process. This allows the City to request mitigation measures to improve the proposal and minimize potential negative impacts.

### JUSTIFICATION

The Local Government Act provides the City with the authority to establish Development Approval Information Areas. The City wishes to use this authority to ensure that developments do not have a negative impact on the natural environment, surrounding properties and the character of the city.

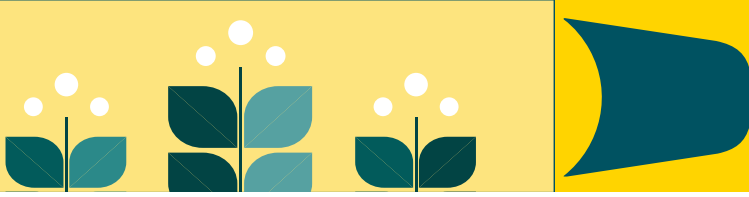
### DEVELOPMENT APPROVAL INFORMATION AREA PROCESS AND REQUIREMENTS

The procedures and policies on the process for requiring development approval information and the substance of the information that may be required are set out in the Development Approval Information bylaw.

## 6.4 CITY CORE

### DESIGNATION

All lands zoned for commercial, industrial, or multifamily uses in the Downtown, Midtown and Marina Districts in the City Core area (see Map 1A and 1B) of the City of Prince Rupert are designated Development Permit Areas pursuant to Section 488 1 (d, f, and j) of the Local Government Act for the purpose of establishing objectives for the development of these lands.



## JUSTIFICATION

Given the critical importance that the City Core has in attracting visitors, providing residents with an attractive and vital urban setting, and attracting new families and workforce, the City Core area and its three sub-districts need to be well designed. The quality of the built environment in the Core area will directly impact future economic development, quality of life and the City's image.

## 6.5 GENERAL MULTIFAMILY AREAS

### DESIGNATION

All lands zoned multifamily outside of the City Core area (see Maps [1A](#) and [1B](#)) are also designated as Development Permit Areas pursuant to Section 488 1 (f) of the Local Government Act for the purpose of establishing objectives for the development of commercial lands.

### JUSTIFICATION AND OBJECTIVES

To encourage multi-family developments that are well designed and enhance Prince Rupert's built environment, all properties zoned and developed for multi-family housing (townhouses and apartments where the site or building contains three or more units) are designated in the Multi-Family Housing Development Permit Area. Applicants within this designation shall include provision for landscaping, building form, and parking layout. Particular attention is to be paid to screening surface parking areas, building orientation to take advantage of views, and interesting architectural treatment of building facades.

## 6.6 GENERAL COMMERCIAL AREAS

### DESIGNATION

All lands zoned commercial outside of the City Core (see Maps [1A](#) and [1B](#)) are also designated as Development Permit Areas pursuant to Section 488 of the Local Government Act for the purpose of establishing objectives for the development of commercial lands.

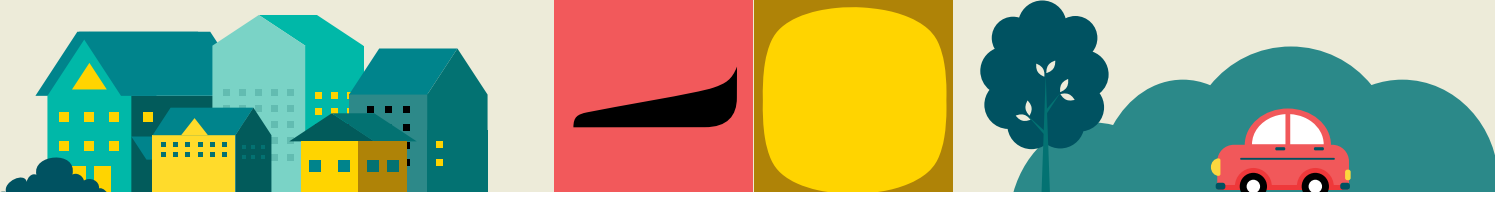
### JUSTIFICATION AND OBJECTIVES

Given that commercial developments are high-profile land uses and influence the character of the City and its neighbourhoods, it is important to carefully guide the development of commercial uses. This designation aims to ensure a high quality of building design and landscaping, to ensure commercial developments are complementary to neighbourhoods and contribute to the city's quality.

## 6.7 INDUSTRIAL AREAS

### DESIGNATION

All lands zoned industrial in the City of Prince Rupert (see Map [1A](#) and [1B](#)) are designated as Development Permit Areas under Section 488 1 (f) of the Local Government Act for the purpose of establishing form and character objectives for the development of industrial lands.



## JUSTIFICATION AND OBJECTIVES

The City of Prince Rupert designates all Industrial land as a Development Permit Area. The justification for this designation is to ensure that the City has the ability to establish conditions on developments such that the form and character of new industrial development are of high quality, and best suited to the surrounding properties and the vision of Prince Rupert as expressed in the OCP. The objectives of this Development Permit area designation are to ensure that new industrial development:

1. Is compatible with surrounding land uses.
2. Complements this OCP's social, economic, and environmental goals.
3. Is constructed to high standards, both material and aesthetic.

## 6.8 ENVIRONMENTALLY SENSITIVE (RIPARIAN) AREAS

### DESIGNATION

The Prince Rupert shown on [Map 9](#) as Riparian is designated as a development permit area pursuant to Section 488 (1) (a) of the Local Government Act. This includes all land within 30 metres of the natural boundary/high water mark of each natural boundary of a watercourse or water body.

### JUSTIFICATION AND OBJECTIVES

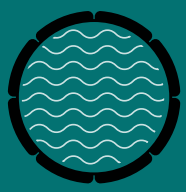
Riparian areas are areas of land that border streams and rivers. They maintain water quality by filtering chemicals and water-borne sediments, providing wildlife corridors and

habitat, providing additional green space, and may assist in flood protection and preventing erosion. Removing vegetation from these riparian areas may increase erosion, destabilize the bank, and permit the introduction of deleterious substances into the watercourse. Increased development in riparian areas provides less surface area for filtration, which may affect fish habitats and reduce water storage capacity. A Development Permit is required to ensure that the ecological value of streams has been considered prior to development, and measures will be taken to mitigate damage to these ecosystems.

In compliance with the Riparian Areas Regulation, enabled by the provincial Fish Protection Act in 2005, the guidelines and regulations below are put in place to protect riparian environments within the City of Prince Rupert.

The objectives of this DPA are to:

1. Preserve and protect environmentally sensitive areas such as streams, wetlands, estuaries, and the foreshore and their supporting upland riparian habitats.
2. Minimize site disturbance and design sites to protect riparian zones and watercourses.
3. Protect riparian areas providing fish habitat, in accordance with the Fish Protection Act, and for the protection of the natural environment and ecosystems that help to protect and maintain the City's watershed.
4. The guidelines described in **Appendix F** are intended to clarify where and how lands might be developed in and around riparian areas.



## 6.9 DEVELOPMENT PERMITS INCLUDING VARIANCES

A Development Permit issued by the City can include variances in zoning, subdivision, signage, and other bylaws, except that land use, density, or flood plain requirements may not be varied. The DP can include a variance provided that the variance would enable the proposed development to comply with a development guideline, or OCP policy or objective. For all other variances, a separate Development Variance Permit will be required.

This designation will enable the City to manage development in steep slope areas in a manner that reduces the risk to life and property, prevents erosion and potential risks to down-slope properties, prevents destabilization of slopes and protects the aesthetic quality of the slopes.

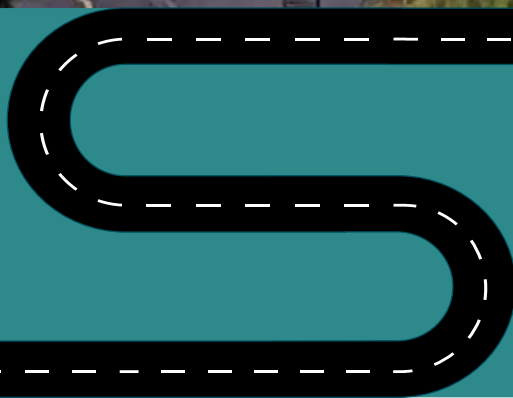
## 6.10 HAZARDOUS AREAS

### STEEP SLOPES DESIGNATION

That part of Prince Rupert shown on [Map 5](#) with slopes of 30 percent or greater is designated as a Development Permit Area pursuant to Section 488 (1) (b) of the Local Government Act. This Development Permit Area includes all areas with slopes exceeding 30% (17.2 degrees) over a minimum six metre run.

### JUSTIFICATION AND OBJECTIVES

Steep topography and thin soil cover make some areas of Prince Rupert susceptible to erosion and possible unstable slopes. Site disturbance, such as land clearing, road construction, changes in slope profiles or construction of buildings, could increase the risk to life and property and harm the environmental value of the slopes.



# IMPLEMENTATION AND MONITORING

An Official Community Plan is a future-oriented document. It speaks to a desired future and is based on the best available information relating to the type and amount of expected growth and the current understanding of the future economy. If economic conditions do not occur as predicted, then the pace and even nature of development may also not occur as assumed. In such cases, it may be that the plan is realized as presented, but just over a longer or possibly a shorter time frame. Or, in the case of major structural changes to society and the economy, the future of the city may need to go in a different direction. The possible different outcomes are reasons why OCPs need to be updated periodically to ensure that assumptions and conditions are still relevant.





In addition to its future-oriented focus, the OCP encourages and enables certain actions to work towards the future vision. It is a legal requirement that all bylaws adopted and works undertaken by the City must be consistent with the OCP. The OCP does not require the City to undertake any action, but these actions must be consistent with the OCP.

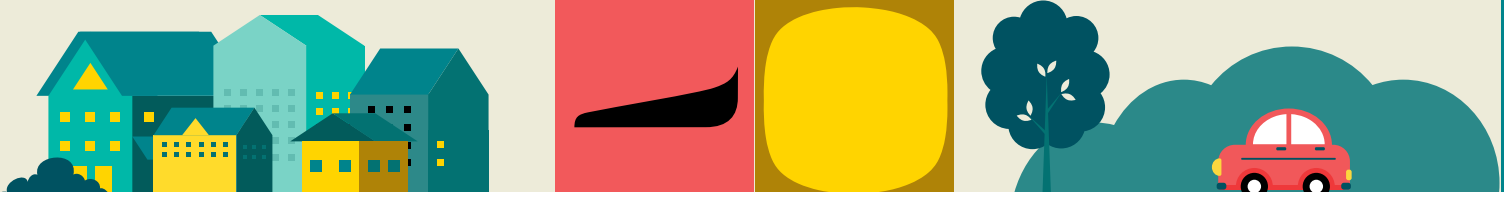
The City's vision is optimistic and aspirational, based on the port industry's growth predictions. The future assumes significant economic growth that is partly dependent on port industry growth and partly upon the City's ability to work with industry and other governments to finance the improvements needed to attract new workers and retain them.

The improvements needed are a refreshed and renewed City Core and a renewed physical infrastructure. The old, and in some cases beyond life expectancy, infrastructure presents a critical challenge to the City and its leaders. This challenge will require significant funding and a strategic phasing of improvements to ensure that works, services and street improvements are renewed efficiently and effectively. For example, renewing streets before the underground services are upgraded would result in the value of those street investments being lost when the underground works and services are repaired or replaced.

Other social and cultural needs are layered on top of the need for renewed infrastructure and a revitalized and attractive downtown. Addressing all of these together will require partnerships, assistance, and private sector support, along with a strategic approach to investments. To develop this strategic approach, the City will need accurate information. This will require investment in additional planning to develop subsequent plans, other bylaws, and amendments to enable the implementation of this OCP.

Organizational capacity is also a critical resource needed to take on the tasks for overall City renewal. Undertaking studies and plans, developing partnerships, administering contracts for developments and construction, and operating and maintaining the City's assets all take financial and human resources. This is a critical point, as expectations from the community, industry, and Council regarding what can be undertaken and operated and the necessary resources are involved. Because of the past economic and population decline trend in the City, there was an associated reduction in the City's human resources. There will need to be an associated increase in this critical resource once the anticipated growth cycle begins. Indeed, the demands associated with preparing the City for this growth, and the demands of responding to an infrastructure that is often well past its serviceable life, may require some additional human resource capacity before the onset of the growth.

The public recognizes that while all the OCP objectives may be important, some are more urgent than others. Early in the consultation, the public was asked to rate the importance and urgency of different City functions. Table 2 displays the results of this input and shows that the public recognizes that core City functions like roads and sewers are both important and urgent.



The results show an understanding of critical social issues like affordable housing. They also show that some areas, while important (e.g., bike paths and cultural services), are not urgent. These results indicate support for a strategic phasing approach to renewal investments. Not everything can be done simultaneously; however, early development of an overall long-term investment strategy may help show recognition and commitment to achieving everything important over time. This overall long-term strategy should identify and prioritize the most important and urgent areas.

**TABLE 2: RELATIVE IMPORTANCE AND URGENCY OF CITY SERVICES**

| City Service to improve or maintain | This is critically important | This is important | This is not important | This is urgent | This is not urgent | Important and urgent |
|-------------------------------------|------------------------------|-------------------|-----------------------|----------------|--------------------|----------------------|
| Recreation services                 | 48                           | 70                | 4                     | 50             | 70                 |                      |
| Cultural services                   | 20                           | 71                | 31                    | 22             | 98                 |                      |
| Sewage collection and treatment     | 92                           | 27                | 4                     | 97             | 25                 | Yes                  |
| Good roads                          | 83                           | 35                | 3                     | 82             | 39                 |                      |
| Parks                               | 36                           | 74                | 11                    | 99             | 23                 | Yes                  |
| Bus transit                         | 21                           | 70                | 29                    | 29             | 92                 |                      |
| Bike paths                          | 29                           | 47                | 46                    | 30             | 92                 |                      |
| Pedestrian paths                    | 54                           | 56                | 13                    | 69             | 53                 | Yes                  |
| A busy and attractive downtown      | 65                           | 49                | 8                     | 83             | 39                 | Yes                  |
| A healthy environment               | 72                           | 48                | 3                     | 84             | 37                 | Yes                  |
| Employment opportunities            | 58                           | 51                | 13                    | 84             | 37                 | Yes                  |
| Economic Development                | 66                           | 47                | 10                    | 75             | 47                 | Yes                  |
| Affordable housing                  | 74                           | 33                | 15                    | 87             | 34                 | Yes                  |
| Diversity of housing options        | 56                           | 46                | 20                    | 68             | 53                 | Yes                  |



## 7.1 FUNDING AND RESOURCING

While the quantum of funding and resources required to implement the vision in this OCP has not been fully determined, it is very large given the considerable urgent and important investments needed in the City's infrastructure and revitalizing the City Core. Property taxes will continue to be critical for resources, but will not be sufficient. The City will need to find and use a range of funding sources and will need to continue to find and develop key partnerships and collaborations to increase its ability to achieve the OCP goals and objectives.

- Property Taxes/Fees and Charges
- Senior Government Funding
- Industry Partnerships
- NGO Partnerships
- Development Cost Charges
- Community Amenity Contributions
- Revenue Generation Through the Sale of Services
- Sale and Leasing of City Property
- Being Entrepreneurial

### PROPERTY TAXES/FEES AND CHARGES

Given the scale of the investments required to return the City's infrastructure to a reliable and good state, Council may consider making annual contributions to an infrastructure fund to work towards a level that will cover a significant portion, if not all, of its current infrastructure gap.

As much as striving for zero tax increases is a popular goal with Local Governments, as an ongoing practice, such fiscal results actually result in smaller real-dollar budgets, making investments in the City and its human resources very difficult, if not improbable, to achieve.

### SENIOR GOVERNMENT FUNDING

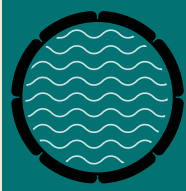
The City of Prince Rupert will likely need significant senior government assistance to fund its infrastructure replacement. City staff have successfully secured some funding; however, such funding is never 100%. Therefore, the City will need access to its own funding when grants are only partial. This underscores the need for the City's own significant infrastructure fund.

### INDUSTRY PARTNERSHIPS

Prince Rupert is fortunate to have a potential partner in the Prince Rupert Port Authority and port-related industries. This partnership was identified in the 2030 Vision process and the resulting strategy. The Prince Rupert Port Authority and its tenants are cognizant of the need to make significant investments in the City to enable the Port and City to attract new workers with their families. This partnership should continue to receive focused attention from Council and the port community.

### NGO PARTNERSHIPS

In addition to working with industry, developing and stewarding partnerships and collaborations with non-government organizations (NGOs) can benefit the city in terms of delivering services. First, this reduces the human resources the City may need to deliver a service. Second, NGOs may be able to bring skills and resources (including volunteer resources) to an initiative that the City does not have. Third, NGOs may be able to access other funding for which the City may not otherwise qualify.



## DEVELOPMENT AND AMENITY COST CHARGES

Given that the City is facing a significant amount of new development and that this will require infrastructure and amenity upgrades, the City should consider adopting Development Cost Charge (DCC) and Amenity Cost Charge (ACC) bylaws to enable the new development to pay its share of the service improvement costs.

### AMENITY COST CHARGES

Because Development Cost Charges (DCCs) are limited to funding core infrastructure such as water, sewer, roads, and drainage, and given the City's need to provide a broader range of amenities to support a growing population, it may be prudent for the City to implement an Amenity Cost Charge (ACC) program. ACCs, recently introduced by the Province of British Columbia, allow local governments to collect funds from new developments to support community amenities such as recreation centres, libraries, childcare facilities, and affordable housing. To encourage a new development cycle in the city, it may be strategic to delay the implementation of ACCs until a few successful developments have been completed. This phased approach would help establish Prince Rupert as a viable and attractive investment destination for developers, while laying the groundwork for a more predictable and transparent amenity funding framework in the future.

## SALE OF CITY PROPERTY

The City owns significant land within its boundaries that can be developed and sold for residential purposes. As market conditions improve and demand increases, this may be a significant funding source for City investments. Where possible, long-term leases should be considered instead of fee simple sales, particularly for industrial land, because the City would benefit over the long term in ongoing rental revenues in addition to the land appreciation.

### BEING ENTREPRENEURIAL

With funding demands being so high and traditional sources limited in ability, the City can and has benefited from entrepreneurial actions. Prince Rupert's actions and results in the ongoing Watson Island reclamation and development initiative are an outstanding example of how a local government can achieve positive outcomes by being creative and entrepreneurial. The administrative and political culture that enables this approach deserves attention to ensure it is fostered and continually supported.



## 7.2 MONITORING

To assist with effective implementation of the Prince Rupert OCP, it is important to measure and monitor the success of implementation actions and resulting effects. To this end, reporting of key development statistics and indicators in the City’s annual report would be helpful. The following indicators are suggested, although these may evolve over time to better track and determine the success of the OCP objectives and implementation efforts.

**TABLE 3: OCP MONITORING FRAMEWORK**

| Monitoring Focus                        | Indicators  | Key Documents / Departments  | Partners  |
|---|---|--|---|
| Port Development                        | Tracking new projects, population growth, and business licensing.   | Economic Development Department<br>Zoning Bylaw<br>Subdivision and Servicing Standard Bylaw        | PRPA<br>Local Indigenous Nations  |
| Development in City Core vs. Greenfield | Tracking the number of new development applications in the city core.   | Complete Communities Assessment<br>Social Development Assessment<br>Connect Rupert<br>Zoning Bylaw | Local Indigenous Nations<br>Chamber of Commerce<br>Market and non-market housing developers and operators |
| Housing Supply                          | Tracking building permits and development permits by building type, the diversity of housing supply by proportion of housing types, and the number of affordable housing units. | Housing Needs Report<br>Housing Acceleration Action Plan   | Local Indigenous Nations<br>Market and non-market housing developers and operators                        |
| Commercial                              | Tracking the number of business licences, assessed value of Class 6 properties, commercial floorspace, and location in downtown and mixed-use areas.                            | Business Licensing<br>Economic Development Department<br>BC Assessment Roll                        | Local Indigenous Nations<br>Chamber of Commerce   |



| Monitoring Focus                                      | Indicators  | Key Documents / Departments  | Partners   |
|---|---|--|--|
| Industrial  | Tracking the number of business licences and assessed value of Class 5 properties.  | Business Licensing<br>Economic Development Department<br>BC Assessment Roll  | Local Indigenous Nations<br>Chamber of Commerce<br>PRPA<br>Proponents                    |
| Parks, Trails & Open Space                            | Tracking the number of hectares of protected passive and active space, distance of developed trails, amount and type of open space protected through new development, and parkland acquisitions as indicators of the community's social health. | Rupert Plays<br>Recreation Plan<br>Operations Department<br>Recreation Department                                      | Local Indigenous Nations<br>Local recreation clubs and organisations                     |
| Infrastructure  | Tracking investment in renewal versus new capital construction, maintenance spending, and frequency of emergency response.  | Capital Plan<br>Operations Plan<br>Asset Management Strategy and 3-Year Roadmap<br>Infrastructure Replacement Strategy | Local Indigenous Nations<br>Provincial and Federal Governments                           |
| Maintenance of Secondary Plans, Strategies and Bylaws | Ensuring all secondary plans, strategies, and bylaws are updated at least once every ten years or as required by legislation.   | Relevant departments and documents   | Local Indigenous Nations<br>Relevant community groups, partners, and government agencies |

## 7.3 TEMPORARY USE PERMITS

The Local Government Act enables the City to designate areas of the City in the OCP for Temporary Use Permits (TUPs) where temporary commercial and industrial uses may be allowed. It also allows the City to specify general conditions regarding the issue of temporary commercial and industrial use permits in those areas.

TUPs allow for the temporary use of land not otherwise permitted in the Zoning Bylaw and may be issued for a period of up to three years with one renewal. Conditions under which a temporary use may be allowed are established in the permit, including the site design and layout, and the length of time the temporary use can occur. Security deposits, site restoration plans and letters of undertaking may also be required to ensure conditions are met.

The entire City is designated for Temporary Use Permits. Temporary use permit applications shall be reviewed with respect to the following conditions:

### GENERAL TUP CONDITIONS

1. Temporary uses must not negatively affect existing business or surrounding properties regarding noise, lighting, parking, traffic, nuisance, or other impacts.
2. An application for a TUP will be considered in relation to:
  - a. Demonstration that the use is temporary
  - b. Surrounding land uses
  - c. Potential conflict with residential land uses
  - d. Potential impacts on environmentally sensitive areas
  - e. Provision of adequate servicing that meets health requirements
  - f. Duration of the proposed temporary use
  - g. Relevant policies within other sections of this plan
3. TUPs may be issued subject to conditions imposed by the City, such as, but not limited to:
  - a. The buildings or structures that may be used
  - b. The period of applicability of the permit
  - c. The area, duration, or timing of use
  - d. Required site rehabilitation upon cessation of the use
4. There must be a strong rationale, including the matters set out in conditions above for a temporary permit instead of an application for a regular zoning change.

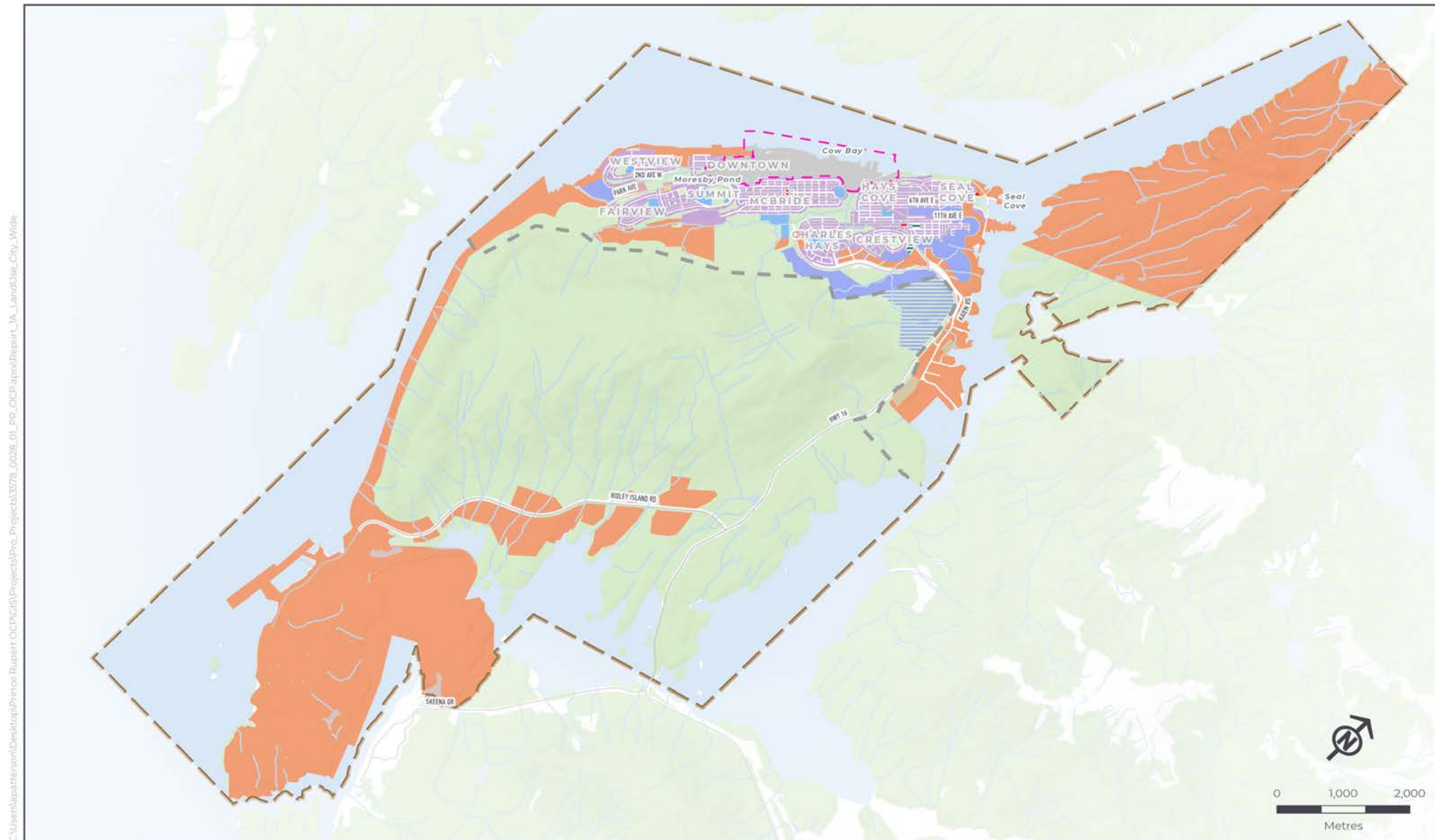
# MAPS:

- **MAP 1A:** City-Wide Land Use Framework
- **MAP 1B:** Townsite Land Use Framework
- **MAP 1C:** City Core Conceptual Land Use Framework
- **MAP 2:** City Core Building Heights Framework
- **MAP 3:** Townsite Parks and Open Spaces Framework
- **MAP 4:** City-Wide Sand and Gravel Quarry Locations
- **MAP 5:** Townsite Slopes 30%
- **MAP 6A:** Townsite Streets Framework
- **MAP 6B:** City Core Streets Framework
- **MAP 6C:** Mobility Hubs
- **MAP 6D:** Priority Pedestrian Network
- **MAP 6E:** Priority Cycling Network
- **MAP 7:** Townsite Water Main Replacements
- **MAP 8:** Urban Containment Boundary and Development Areas
- **MAP 9:** Riparian DP Areas

# APPENDICES:

- **Appendix A:** City Core Development Permit Guidelines for Commercial, Industrial and Multi-Family Development
- **Appendix B:** Development Permit Guidelines Multifamily Areas
- **Appendix C:** Development Permit Guidelines, General Commercial Areas
- **Appendix D:** Development Permit Guidelines, Industrial Areas
- **Appendix E:** Development Permit Guidelines, Riparian Areas
- **Appendix F:** Development Permit Guidelines, Hazardous Areas

# MAP 1A: CITY-WIDE LAND USE FRAMEWORK



**Land Use**

- Local Retail/Mixed Use
- Large Format/Service Retail/Mixed Use
- Residential

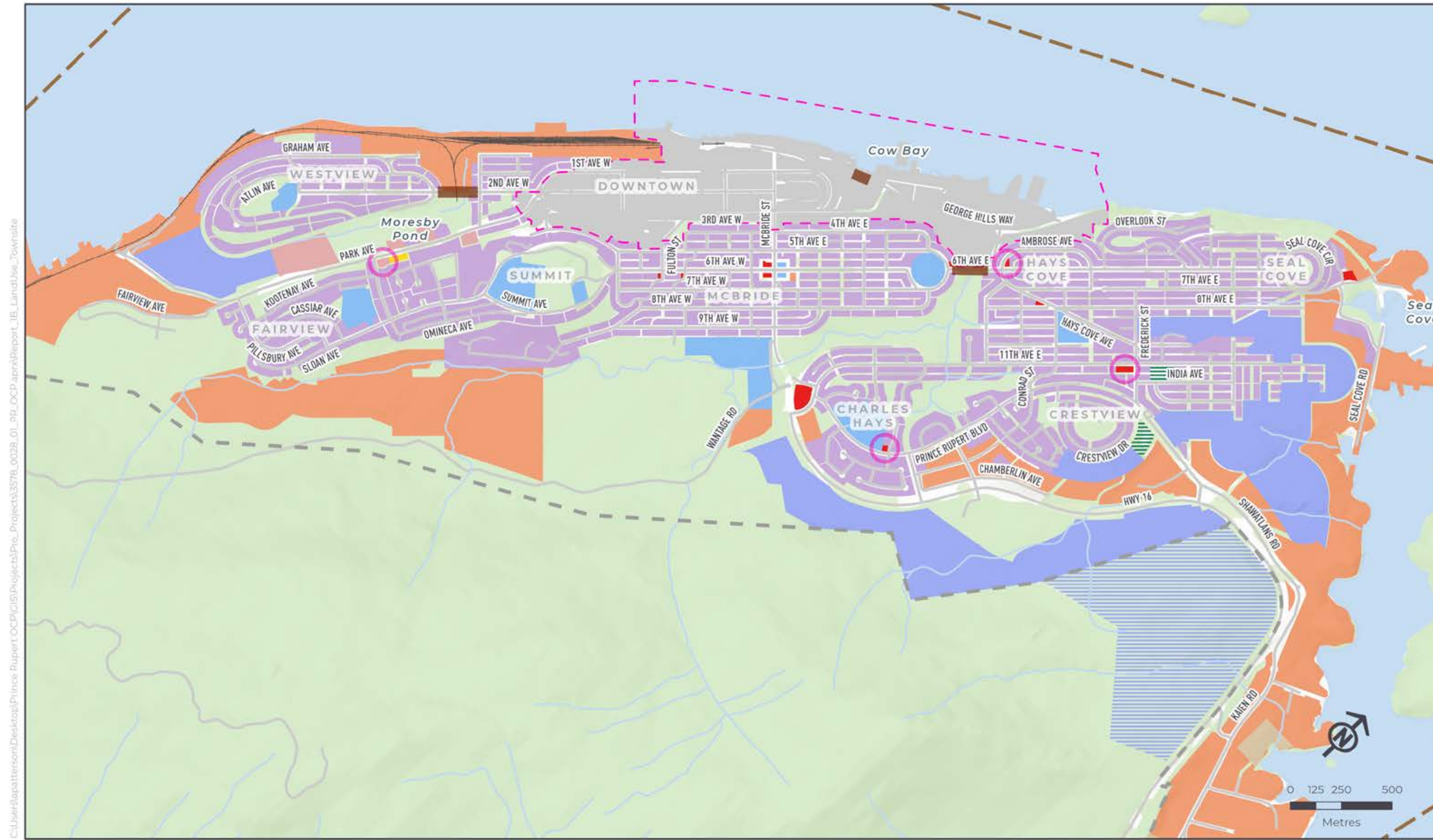
- Industrial
- Civic/Community
- Park/Open Space
- Future Neighbourhoods

- Residential/Industrial Flex
- Potential New Park Location
- Hotel

- Urban Containment Area
- City Core
- Municipal Boundary
- First Nation Reserve



# MAP 1B: TOWNSITE LAND USE FRAMEWORK



**Land Use**

- Local Retail/Mixed Use
- Large Format/Service Retail/Mixed Use
- Residential

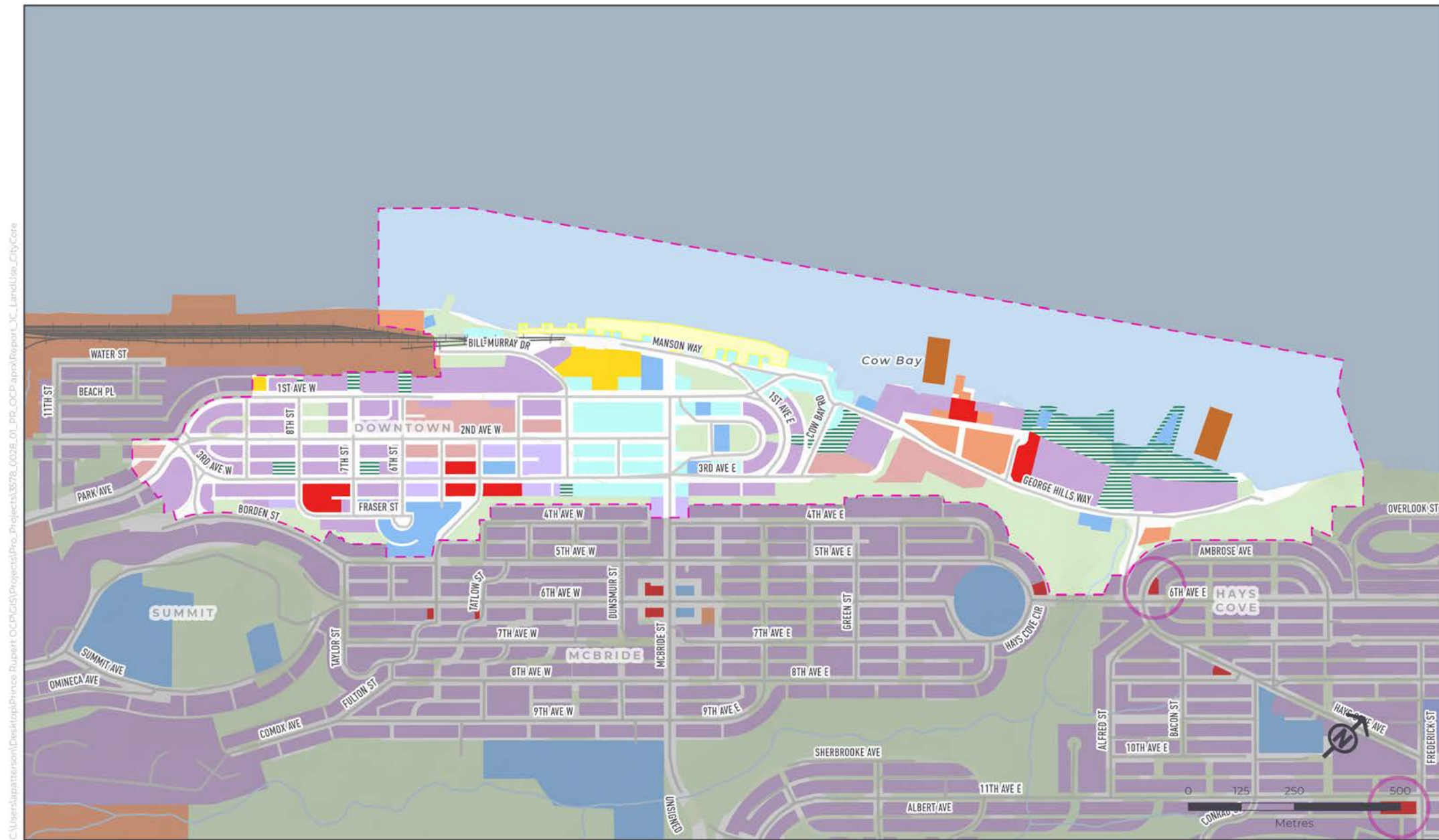
- Industrial
- Civic/Community
- Park/Open Space
- Future Neighbourhoods

- Residential/Industrial Flex
- Potential New Park Location
- Hotel

- Wooden Trestle Bridge
- Urban Containment Area
- Neighbourhood Centre
- City Core
- Municipal Boundary
- First Nation Reserve



# MAP 1C: CITY CORE CONCEPTUAL LAND USE FRAMEWORK



**Land Use**

- Local Retail/Mixed Use
- Large Format/Service Retail/Mixed Use
- Residential
- Industrial

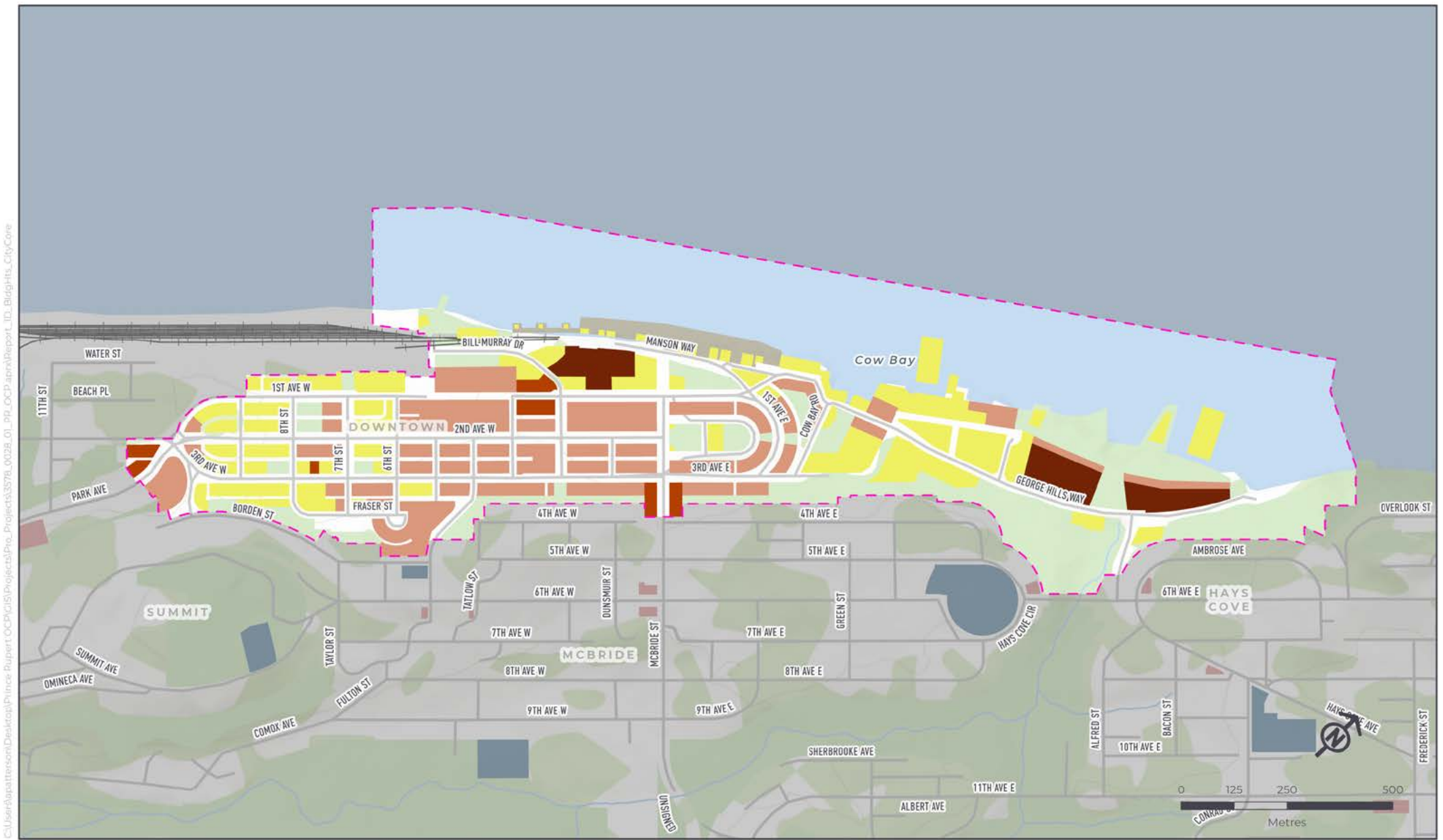
- Civic/Community
- Park/Open Space
- Future Neighbourhoods
- Residential/Industrial Flex
- Potential New Park Location

- CBD Retail/Mixed Use
- Floating Village
- Hotel
- Office
- Public Dock

- Railway
- Neighbourhood Centre
- City Core



# MAP 2: CITY CORE BUILDING HEIGHTS FRAMEWORK



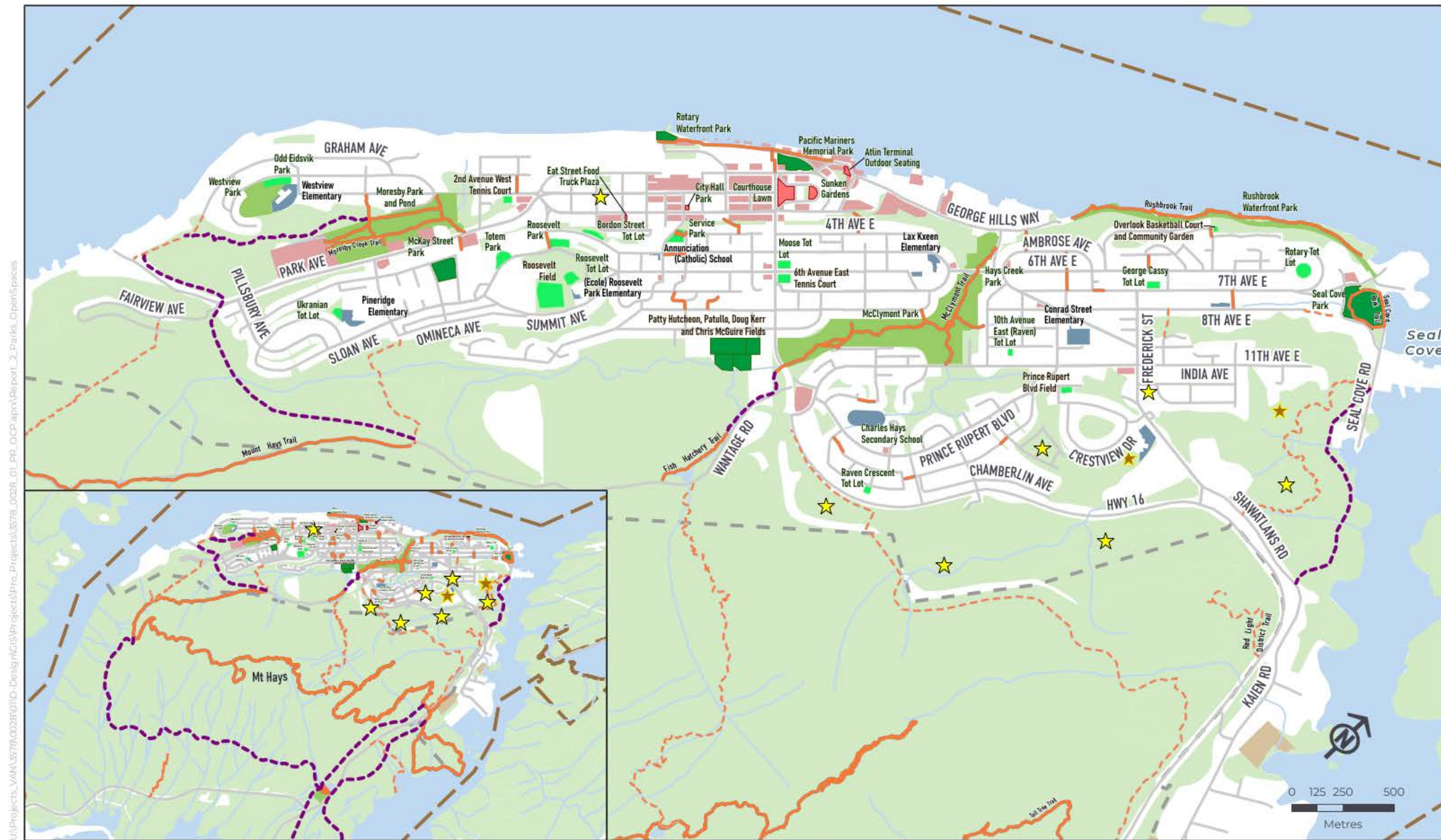
## Downtown Core Building Heights

- 1-2 Storeys
- 3-4 Storeys
- 5-6 Storeys
- 7-8 Storeys

- Railway
- City Core
- School
- Public Dock
- Commercial / Mixed Use
- Park / Open Space / Forested Area



# MAP 3: TOWNSITE PARKS AND OPEN SPACES FRAMEWORK



## Parks & Open Space

- Community Park
- Urban Open Space
- Neighbourhood Park
- Natural Park
- School Site

- Proposed Community Park
- Proposed Neighbourhood Park

## Trails

- Designated Trail
- Non-designated Trail
- Proposed Trail








- Urban Containment Area
- Municipal Boundary
- Commercial / Mixed Use
- Park / Open Space / Forested Area
- First Nation Reserve



# MAP 4: CITY-WIDE SAND AND GRAVEL QUARRY LOCATIONS



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-  Sand or Gravel Quarry
-  Commercial / Mixed Use
-  Railway
-  Park / Open Space / Forested Area
-  Urban Containment Area
-  First Nation Reserve
-  Municipal Boundary



# MAP 5: TOWNSITE SLOPES 30%

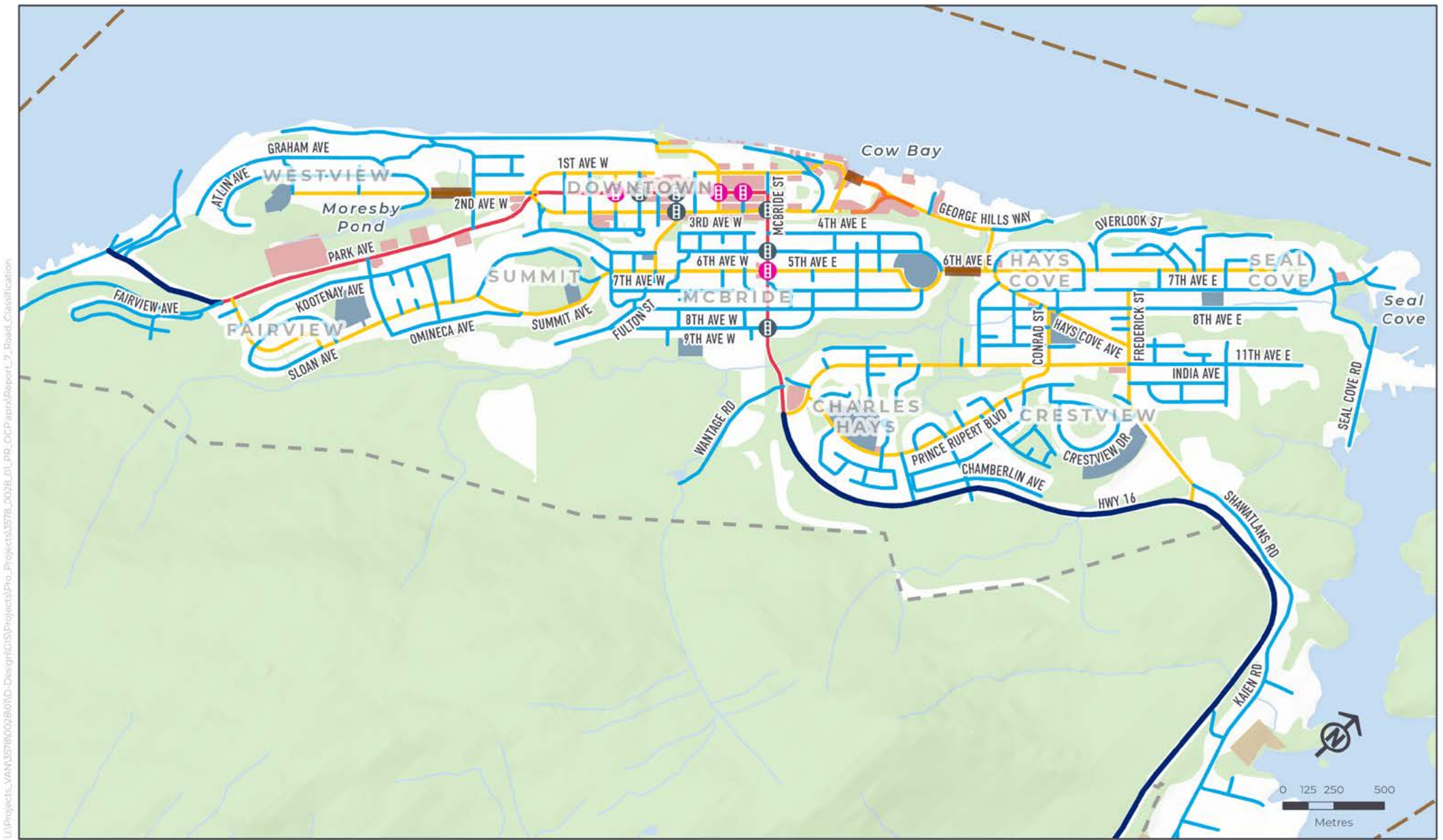


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- Slope > 30%
- Commercial / Mixed Use
- Park / Open Space / Forested Area
- School
- First Nation Reserve
- Urban Containment Area
- Municipal Boundary



# MAP 6A: TOWNSITE STREETS FRAMEWORK

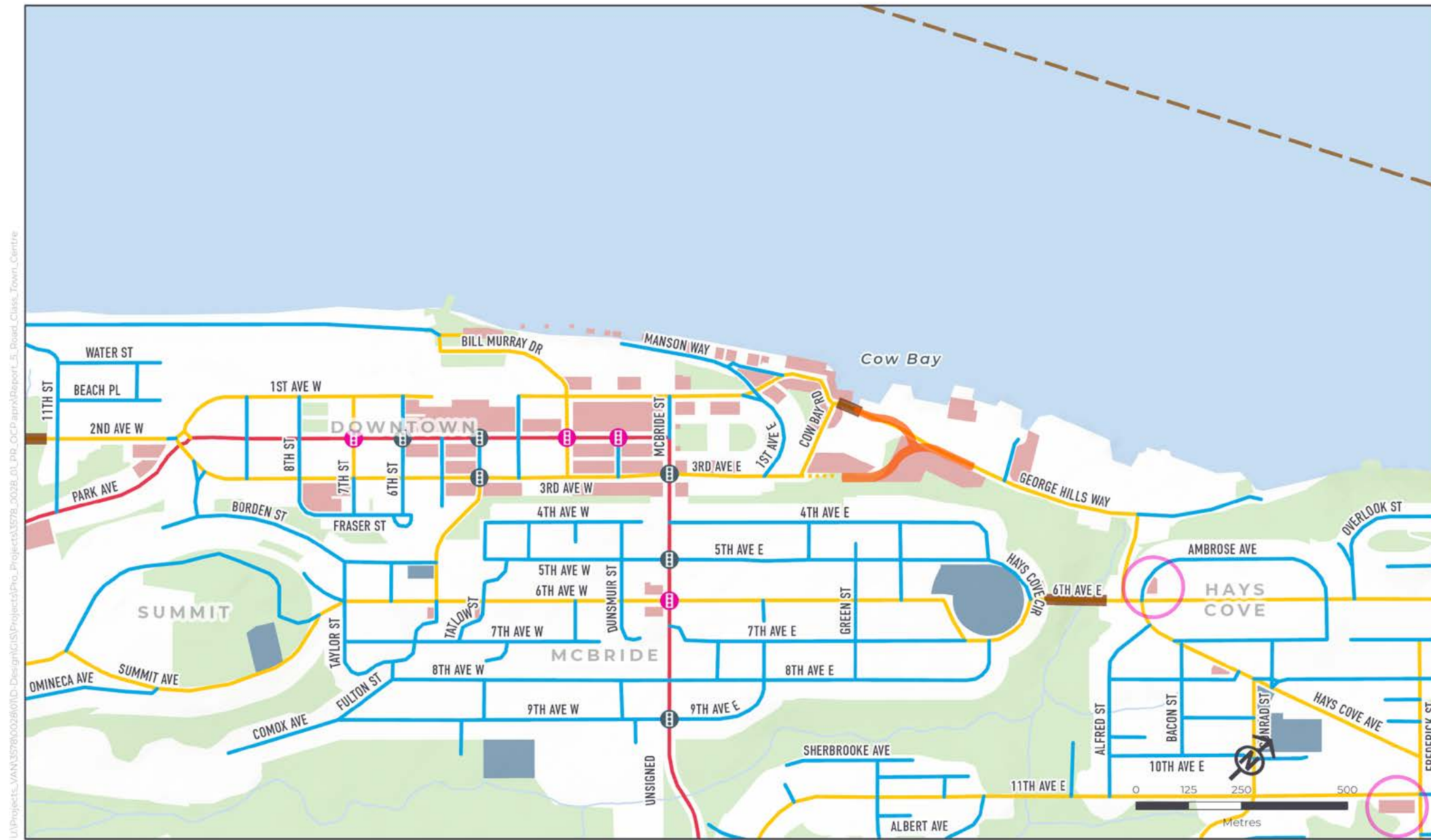


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- |                         |                        |                                   |
|-------------------------|------------------------|-----------------------------------|
| Existing Traffic Signal | Third Avenue Extension | Commercial / Mixed Use            |
| Proposed Traffic Signal | Wooden Trestle Bridge  | Park / Open Space / Forested Area |
| Highway                 | Urban Containment Area | School                            |
| Arterial                | Municipal Boundary     | First Nation Reserve              |
| Collector               |                        |                                   |
| Future Collector        |                        |                                   |
| Local                   |                        |                                   |



# MAP 6B: CITY CORE STREETS FRAMEWORK



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- |                         |                        |                                   |
|-------------------------|------------------------|-----------------------------------|
| Existing Traffic Signal | Third Avenue Extension | Commercial / Mixed Use            |
| Proposed Traffic Signal | Wooden Trestle Bridge  | Park / Open Space / Forested Area |
| Arterial                | Municipal Boundary     | School                            |
| Collector               | Neighbourhood Centre   | First Nation Reserve              |
| Future Collector        |                        |                                   |
| Local                   |                        |                                   |



# MAP 6C: MOBILITY HUBS

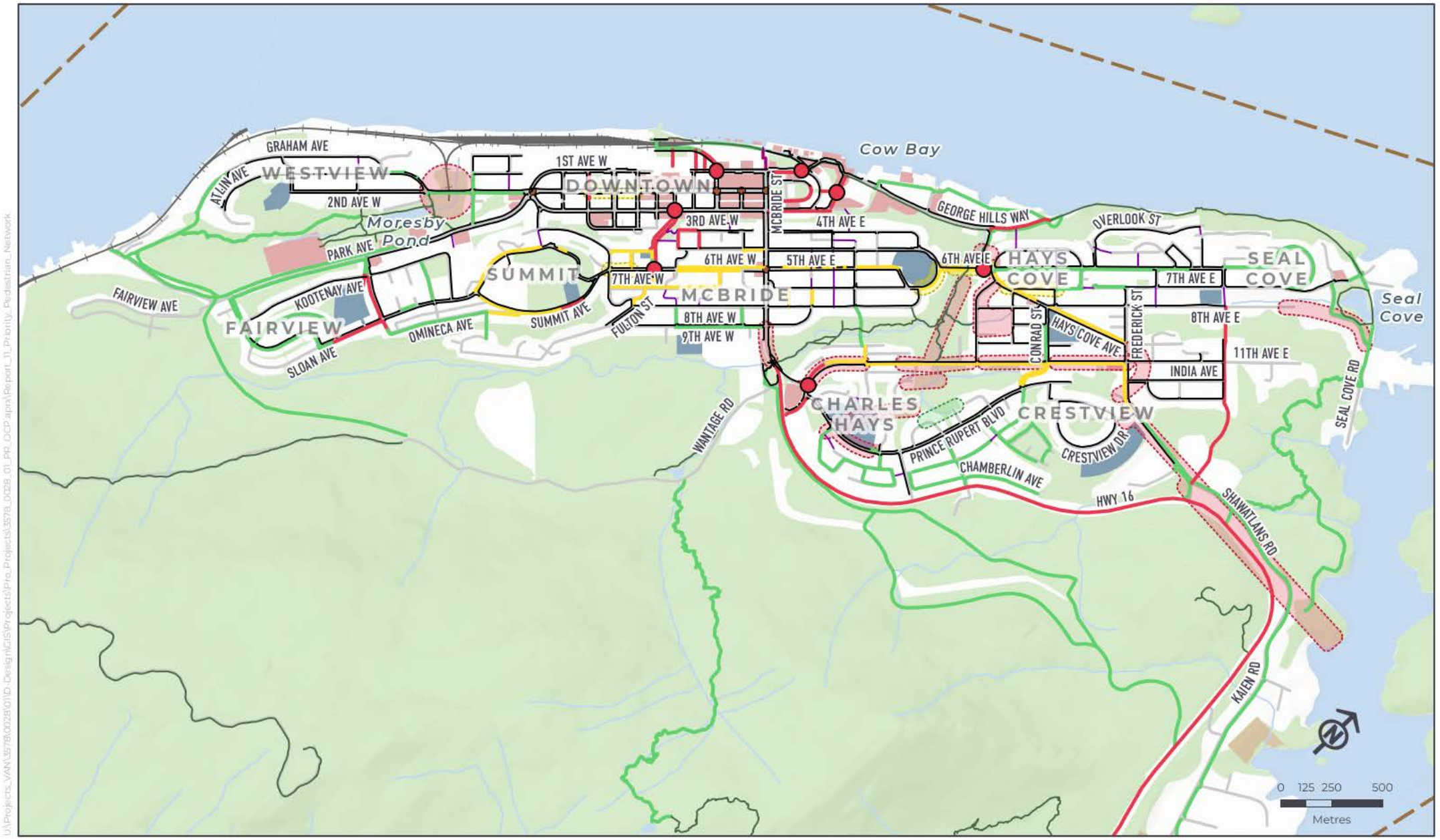


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- Mobility Hub
- Mobility Hub Buffer (400 m)
- Ferry Terminal
- Recreation Facility
- Museum of Northern British Columbia
- Prince Rupert Airport
- Prince Rupert City Hall
- Prince Rupert Public Library
- Prince Rupert Regional Hospital
- Commercial / Mixed Use
- Park / Open Space / Forested Area
- School
- First Nation Reserve



# MAP 6D: PRIORITY PEDESTRIAN NETWORK

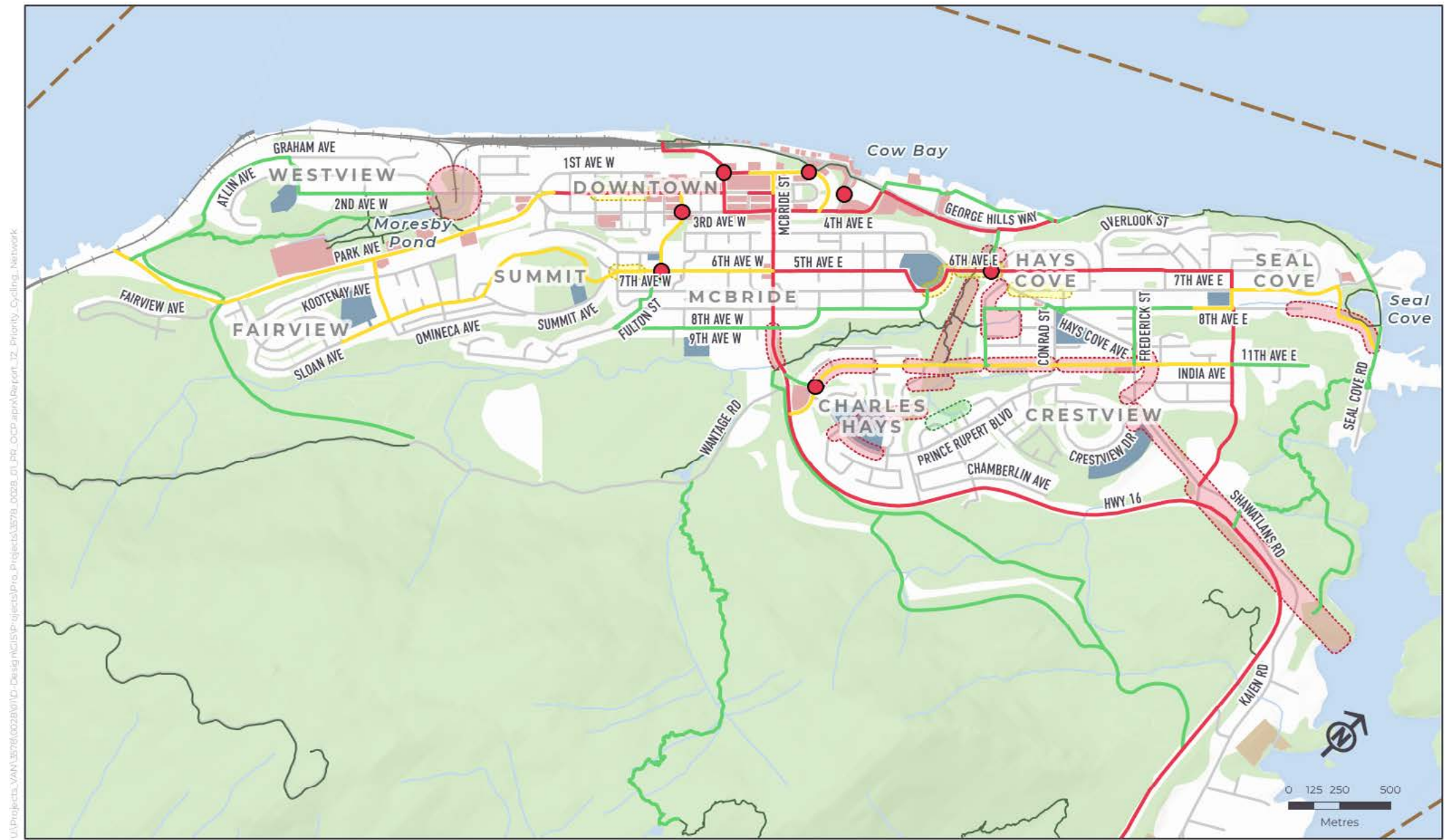


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|--|--|---|
| <ul style="list-style-type: none"> <li><span style="color: red;">●</span> Pedestrian Safety and Placemaking Improvement Site - Short Term Priority</li> <li><span style="color: red;">—</span> Short Term Priority</li> <li><span style="color: yellow;">—</span> Medium Term Priority</li> <li><span style="color: green;">—</span> Long Term Priority</li> </ul> | <ul style="list-style-type: none"> <li><span style="border: 1px dashed red; display: inline-block; width: 15px; height: 10px;"></span> Short Term Priority</li> <li><span style="border: 1px dashed yellow; display: inline-block; width: 15px; height: 10px;"></span> Medium Term Priority</li> <li><span style="border: 1px dashed green; display: inline-block; width: 15px; height: 10px;"></span> Long Term Priority</li> <li><span style="border-bottom: 1px solid black; display: inline-block; width: 20px;"></span> Existing Sidewalk</li> <li><span style="border-bottom: 1px solid purple; display: inline-block; width: 20px;"></span> Existing Walking Path / Stairs</li> <li><span style="border-bottom: 1px solid black; display: inline-block; width: 20px;"></span> Existing Trail</li> </ul> | <ul style="list-style-type: none"> <li><span style="border-bottom: 1px solid black; display: inline-block; width: 20px;"></span> Railway</li> <li><span style="border: 1px solid black; display: inline-block; width: 20px; height: 10px;"></span> Municipal_Boundary</li> <li><span style="background-color: #c0392b; display: inline-block; width: 15px; height: 10px;"></span> Commercial / Mixed Use</li> <li><span style="background-color: #8bc34a; display: inline-block; width: 15px; height: 10px;"></span> Park / Open Space / Forested Area</li> <li><span style="background-color: #3498db; display: inline-block; width: 15px; height: 10px;"></span> School</li> <li><span style="background-color: #d35400; display: inline-block; width: 15px; height: 10px;"></span> First Nation Reserve</li> </ul> |
|--|--|---|



# MAP 6E: PRIORITY CYCLING NETWORK

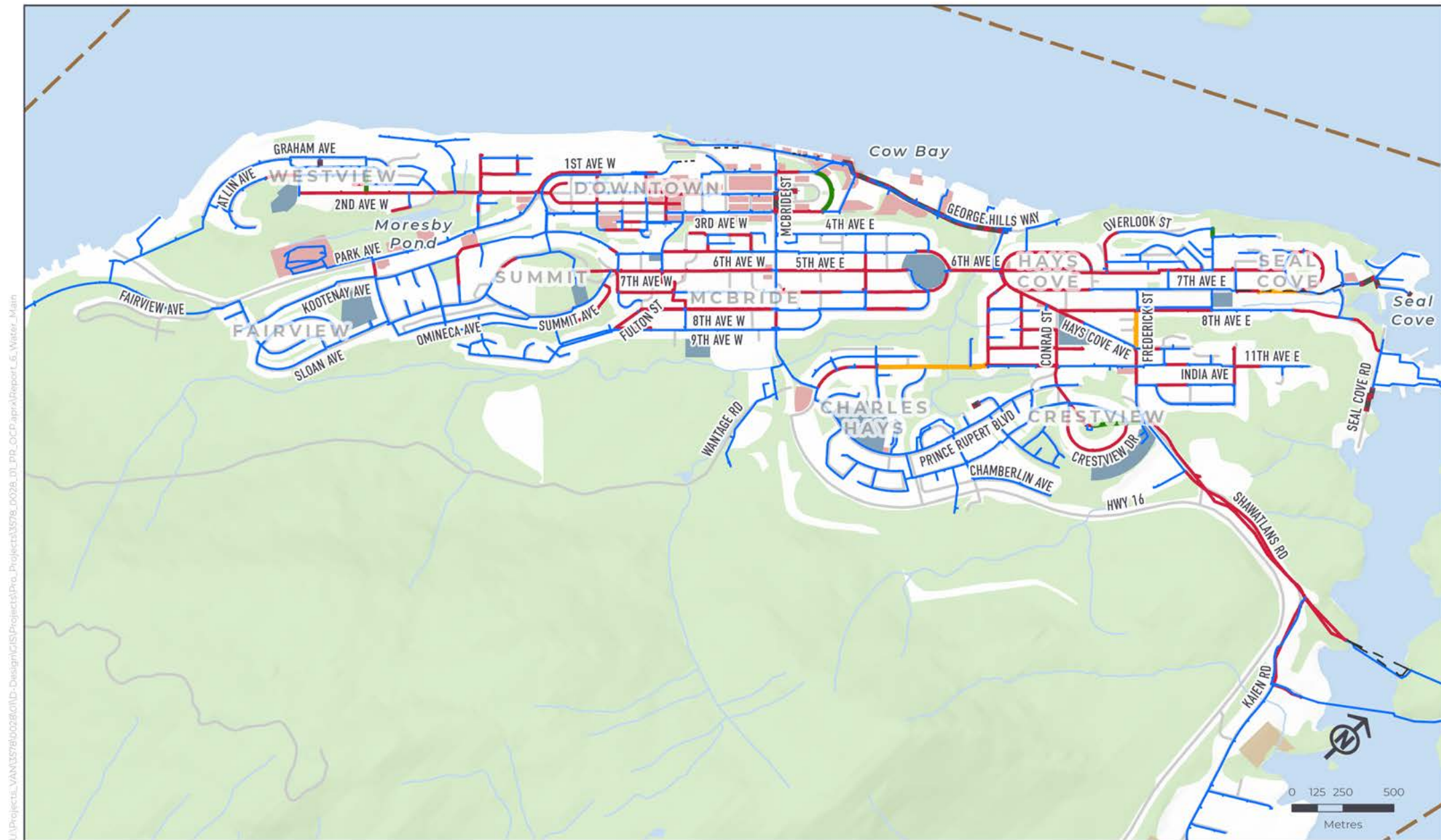


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| <ul style="list-style-type: none"> <li><span style="color: red;">●</span> Pedestrian Safety and Placemaking Improvement Site - Short Term Priority</li> <li><span style="border-bottom: 2px solid red; width: 20px; display: inline-block;"></span> Short Term Priority</li> <li><span style="border-bottom: 2px solid yellow; width: 20px; display: inline-block;"></span> Medium Term Priority</li> <li><span style="border-bottom: 2px solid green; width: 20px; display: inline-block;"></span> Long Term Priority</li> </ul> | <p><b>Conditions Assessment Priority Corridor</b></p> <ul style="list-style-type: none"> <li><span style="border: 1px dashed red; width: 20px; height: 10px; display: inline-block;"></span> Short Term Priority</li> <li><span style="border: 1px dashed yellow; width: 20px; height: 10px; display: inline-block;"></span> Medium Term Priority</li> <li><span style="border: 1px dashed green; width: 20px; height: 10px; display: inline-block;"></span> Long Term Priority</li> <li><span style="border-bottom: 1px solid black; width: 20px; display: inline-block;"></span> Existing Trail</li> </ul> | <ul style="list-style-type: none"> <li><span style="border-bottom: 1px solid black; width: 20px; display: inline-block;"></span> Railway</li> <li><span style="border: 1px solid black; width: 20px; height: 10px; display: inline-block;"></span> Municipal Boundary</li> <li><span style="background-color: #e08080; width: 20px; height: 10px; display: inline-block;"></span> Commercial / Mixed Use</li> <li><span style="background-color: #c8e6c9; width: 20px; height: 10px; display: inline-block;"></span> Park / Open Space / Forested Area</li> <li><span style="background-color: #d7ccc8; width: 20px; height: 10px; display: inline-block;"></span> First Nation Reserve</li> <li><span style="background-color: #546e7a; width: 20px; height: 10px; display: inline-block;"></span> School</li> </ul> |
|---|--|---|



# MAP 7: TOWNSITE WATER MAIN REPLACEMENT



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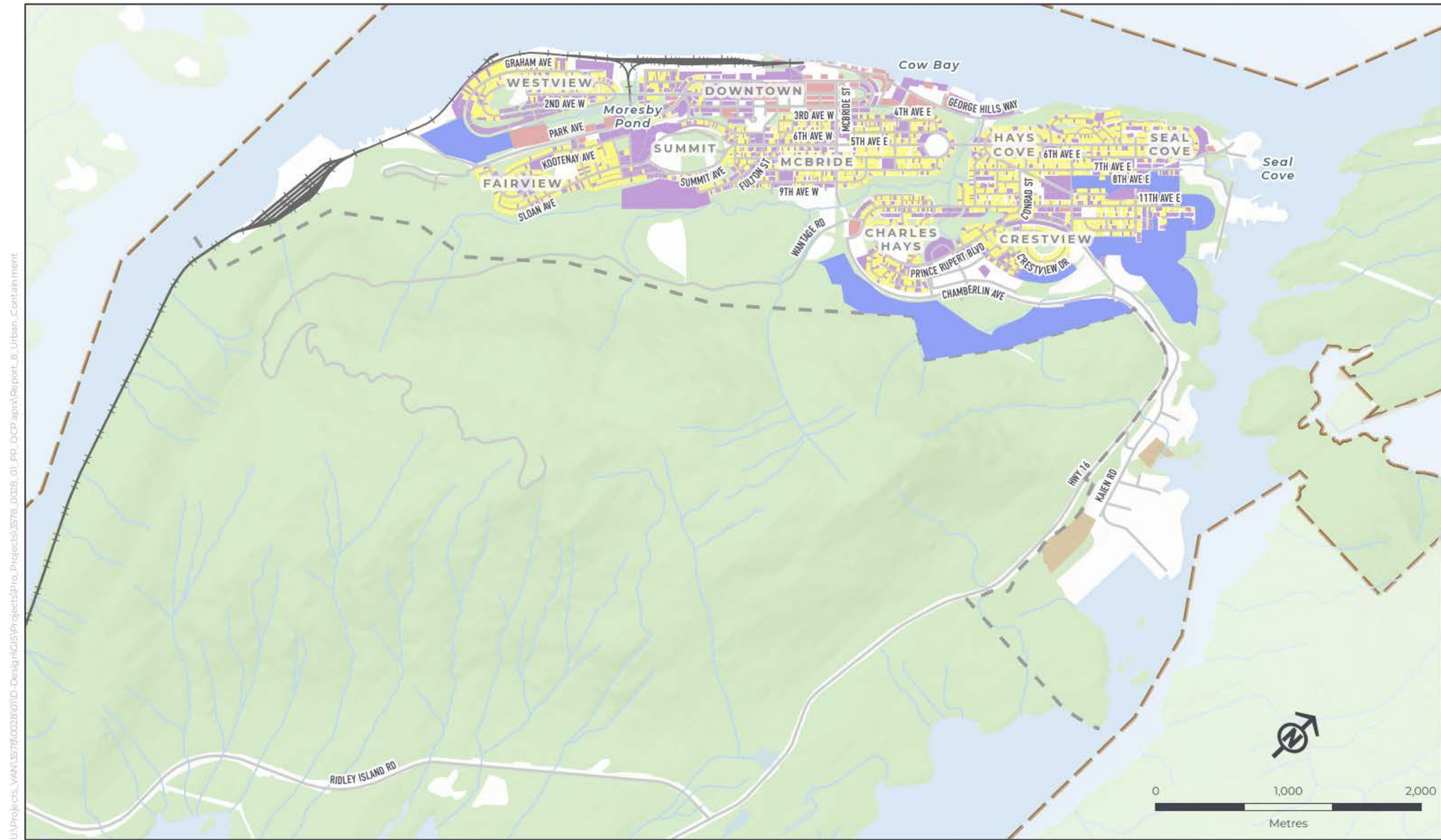
## Status

- Replacement Completed
- Additional Replacement
- High LOF or ROF Removed
- High LOF or ROF
- Existing
- - - Abandoned

- Municipal Boundary
- Commercial / Mixed Use
- Park / Open Space / Forested Area
- School
- First Nation Reserve



# MAP 8: URBAN CONTAINMENT BOUNDARY AND DEVELOPMENT AREAS



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- |   |  |
|---|--|
|  Potential Infill Capacity         |  Railway                |
|  Future Neighbourhoods             |  Urban Containment Area |
|  Residential                       |  Municipal_Boundary     |
|  Park / Open Space / Forested Area |  First Nation Reserve   |



# MAP 9: RIPARIAN DP AREAS

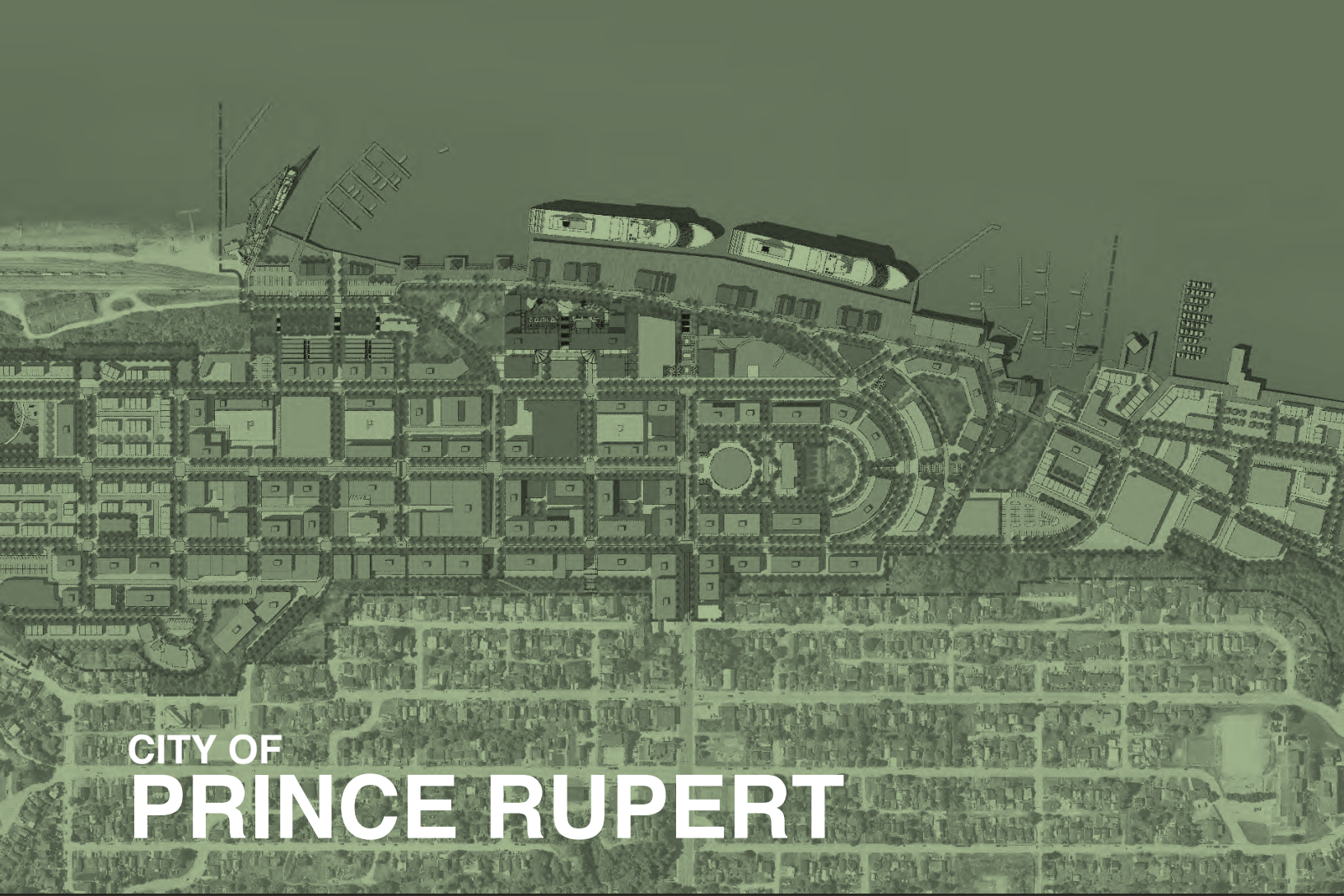


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- Water Course
- Riparian DPA
- Municipal Boundary
- Commercial / Mixed Use
- Park / Open Space / Forested Area
- School
- First Nation Reserve







CITY OF  
**PRINCE RUPERT**

# **CITY CORE DEVELOPMENT PERMIT GUIDELINES**

**FOR COMMERCIAL, INDUSTRIAL AND MULTIFAMILY DEVELOPMENT**

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# INTRODUCTION

These City Core Development Permit Guidelines for Commercial, Industrial and Multifamily Development apply to the Midtown, Downtown and Waterfront Districts within the Development Permit Area. They seek to create beautiful public spaces, vibrant streets and dynamic architecture by defining time-tested strategies and guidelines for new development.

# INTRODUCTION

## How to Use the Guidelines

These City Core Development Permit Guidelines for Commercial, Industrial and Multifamily Development apply to all development within the City Core, unless specifically exempted in Section XX. The City Core is made up of three distinct Districts: Midtown, Downtown and Marina.

The guidelines are organized into three parts:

### GENERAL GUIDELINES

The general guidelines apply to all development within the City Core Development Permit Area. They establish the basic principles of good site and building design for any development application.

### DISTRICT GUIDELINES

The district guidelines are additional guidelines to the general guidelines that apply to development within each specific district. Each district has a different character.

### CHARACTER AREA GUIDELINES

The character area guidelines are additional guidelines to the general and district guidelines that apply to development within each specific character area.

- 1 MIDTOWN
- 2 DOWNTOWN
- 3 MARINA

- 1 HERITAGE CORE
- 2 CIVIC NODE
- 3 COW BAY
- 4 WATERFRONT

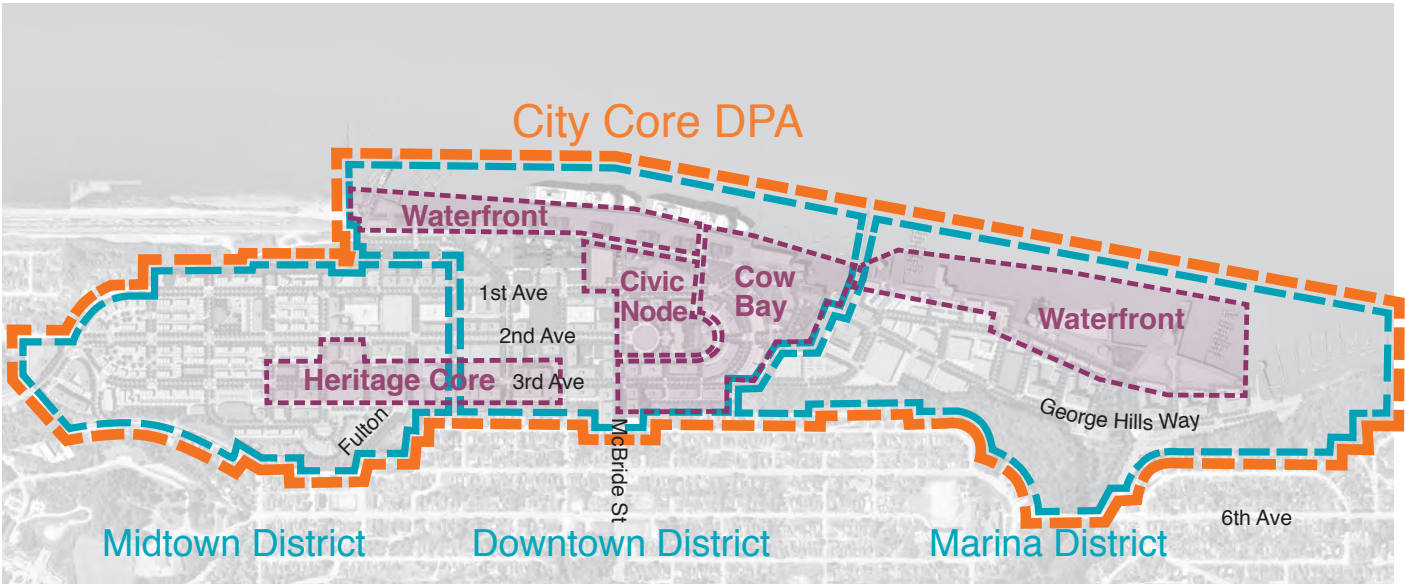


Figure 1: City Core Development Permit Guidelines for Commercial, Industrial and Multifamily Development Areas of Applicability. Refer to each district and character area for greater detail.

---

## Foundation

These City Core Development Permit Guidelines for Commercial, Industrial and Multifamily Development support the design and city-building objectives of the Prince Rupert: City of Rainbows Official Community Plan (OCP) (2020).

They are an evolution of the design guidelines that have previously applied, including:

- Downtown Development Permit Area Design Guidelines (formerly Schedule C to Bylaw No. 3286)
- Cow Bay Development Permit Area Design Guidelines (formerly Schedule B to Bylaw No. 3286)

Many of the core principles of those documents have been carried forward into these design guidelines, including graphic examples.

These design guidelines also reinforce Prince Rupert 2030 The Vision. The demonstration plans used in these design guidelines are from Prince Rupert 2030 The Vision, and serve to illustrate the intended urban form of the City Core.

---

## Interpretation

These guidelines are meant to achieve a balance between compatibility with the existing urban fabric and traditional design principles, while enabling individual expression in new developments. There are many ways to achieve this balance. The guidelines enable a range of strategies for creating buildings that reinforce the vision for the City Core, based on principles of good urbanism and architectural design.

Every building site is unique. Their location, street frontage, adjacent buildings and uses, accessibility and visibility, and historic considerations vary site by site. As well, each new building or building addition will have its own functional programme such as the types of uses, building area, building height, parking and access requirements. Some guidelines will be more important than others depending on the specific context and programme of each building.

It is crucial to interpret these guidelines with flexibility and encourage innovation. While the guidelines provide time-tested strategies for achieving the principles of good urbanism and architectural design, alternative approaches – even breaking the rules – may be appropriate for exceptional new designs.

## Urban Design Principles

Urban design is the comprehensive and cohesive integration of buildings, streets, and open spaces to create liveable places/environments. The essence of good urbanism is determined by the organization of these elements and the relationship between the public and private realm at ground level.

Buildings must face onto streets and public spaces with doors and windows which invite interaction between indoor and outdoor uses, provide casual observation of space, and facilitate direct pedestrian movement and activity. Pedestrian-oriented buildings, streets, squares, and plazas are essential elements to the creation of a functional, aesthetic and vibrant City Core.

Within the City Core, there are six principles of urban design that the public and private realms should work together to achieve.

### 1 Make Connections

Ensure new development reinforces a fine grained pattern of streets and small blocks, including re-establishing the grid network of the 1912 Brett and Hall Town Plan.



### 2 Define Gateways & Entrances

Create a sense of entry at key gateways into the downtown.



### 3 Make Streets for People

Ensure active and visually interesting building fronts to create vitality, comfort and safety for pedestrians.



### 4 Protect Public Views

Preserve and restore significant view corridors to the harbour, prominent buildings and landscape features.



### 5 Protect from the Elements

Incorporate weather protection to enhance pedestrian comfort.



### 6 Foster a Unique Sense of Place

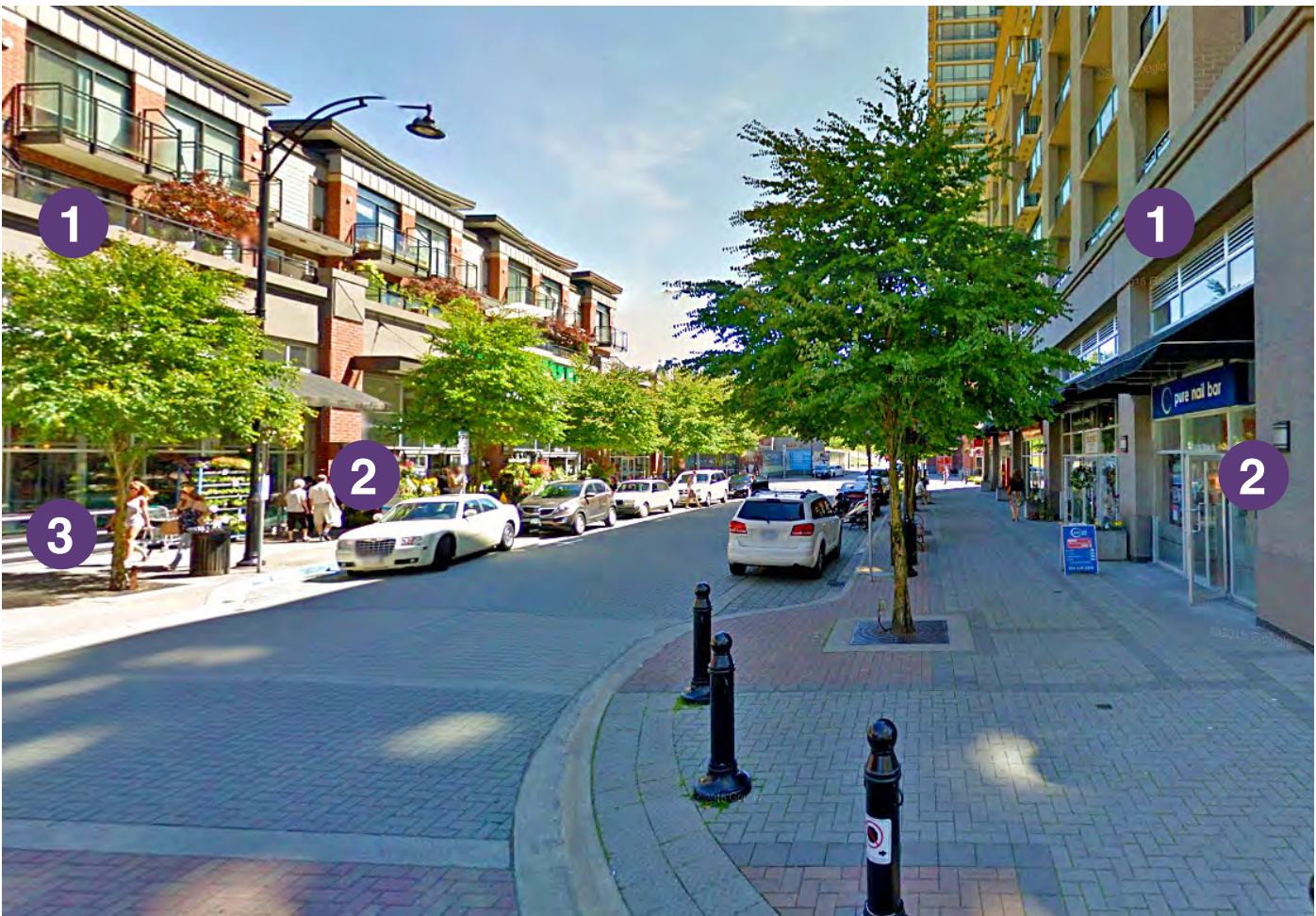
Design buildings that are rooted in the history and cultural identity of Prince Rupert.



# DESIGN PRINCIPLES

In order to support the development of vibrant and active streets, healthy neighbourhoods, and a sense of community, these guidelines are premised on the following three simple yet fundamental approaches to the design of the private realm:

- 1** Build to the sidewalk to create a sense of enclosure, immediacy and continuity along the street.
- 2** Make the street front visually and physically permeable with doors and windows to active uses at ground level.
- 3** Design at a pedestrian scale, including by locating off-street parking behind or under the building.





# GENERAL GUIDELINES FOR ALL DISTRICTS

The general guidelines outline the fundamental strategies for achieving site and architectural designs that are pedestrian scaled, create great streets and public spaces, and establish a building stock that contributes to a great City. The general guidelines apply to all development with commercial or multifamily residential uses, including mixed uses.

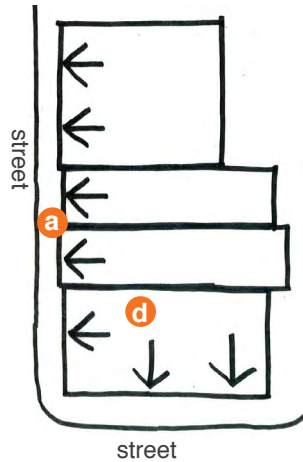
# BUILDING PLACEMENT & ORIENTATION

## Objective

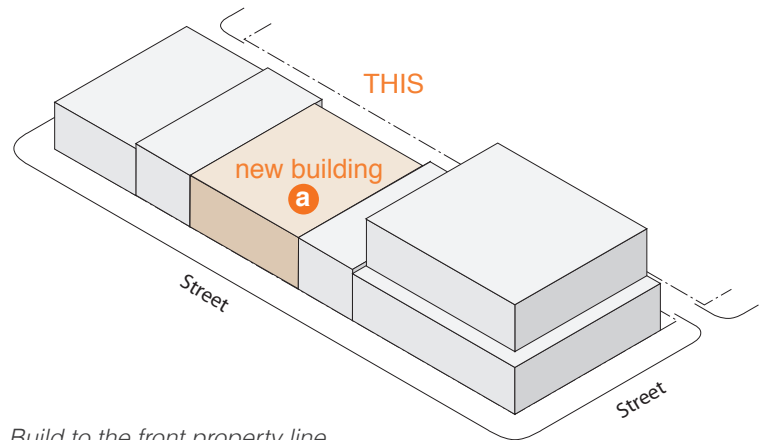
- » To locate and design buildings to frame and define streets and other public open spaces.
- » To arrange buildings in response to their context and specific site conditions and opportunities.

## Guidelines

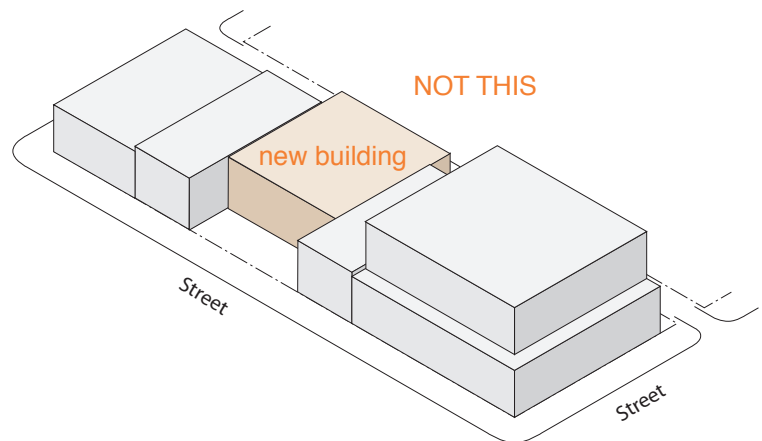
- Locate buildings at or near the front property line to create good street definition and a sense of enclosure.
- Along streets with an established building setback, new buildings should be placed to fit within the predominant setback of the block.
- No parking is permitted between any building and the street edge.
- Orient the primary facade to face the street or adjacent public space.
- Buildings may be set back from the street edge where they frame and define the edges of public space, such as plazas, courtyards, seating areas and enhanced sidewalks; or, where it provides a view to an important landmark.

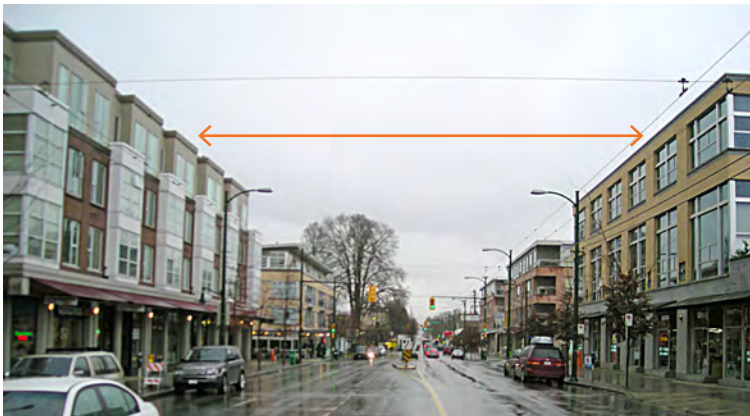
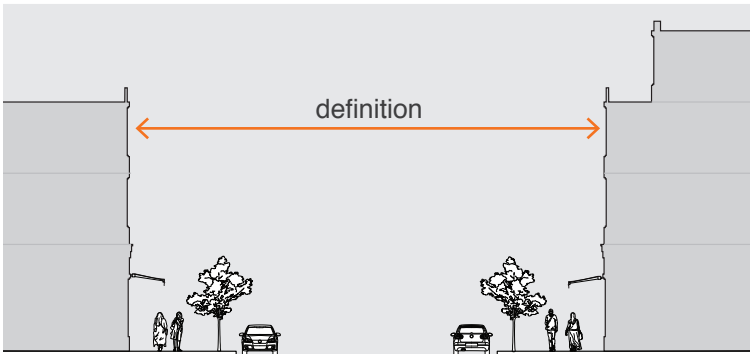


*Buildings should be oriented towards and address public streets and open spaces.*

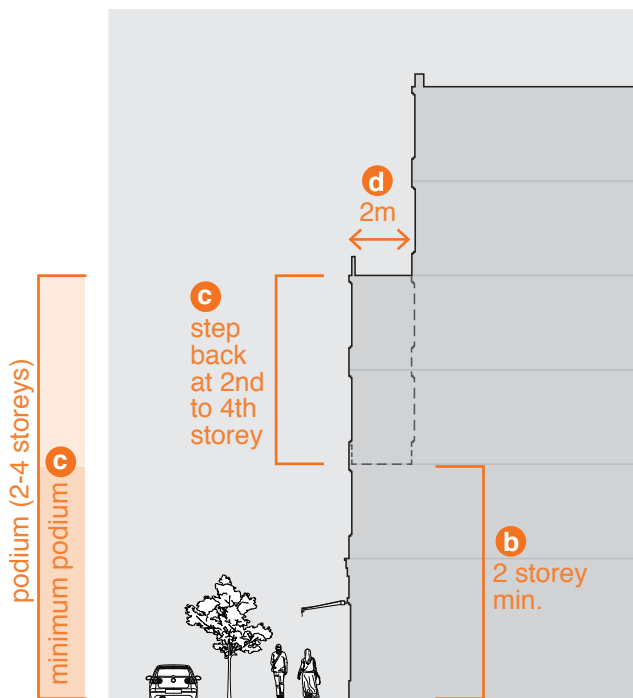


*Build to the front property line.*





Great streets require definition. This is achieved by building to the sidewalk, building continuously along the frontage, and creating a pedestrian scaled street wall.



2m step back above the 2nd, 3rd, or 4th storey for buildings of 5 or more storeys.

## Objective

- » To create continuity in the building edges that define streets and public spaces.
- » To reduce the visual mass of large, single buildings.
- » To ensure pedestrians have access to light and sky views from sidewalks.

## Guidelines

- Buildings should be continuous along their frontages at podium level. Side yard setbacks and gaps are generally discouraged, except where required for mid-block pedestrian connections or access. See District Guidelines for specific percentages.
- Building heights should be a minimum of 2 storeys.
- Building height at the street wall should be between 2 and 4 storeys. The street wall height of the building is referred to as the podium.
- Buildings of 5 or more storeys should provide a minimum 2 metre step back above the podium.

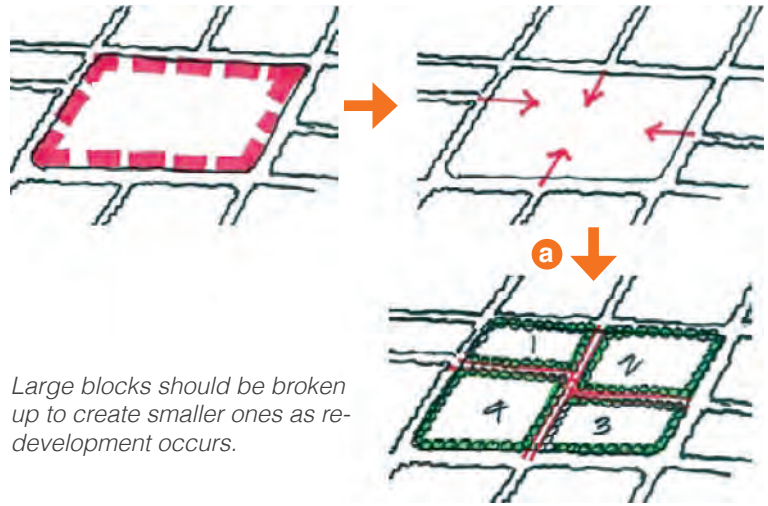
# PEDESTRIAN GRAIN

## Objective

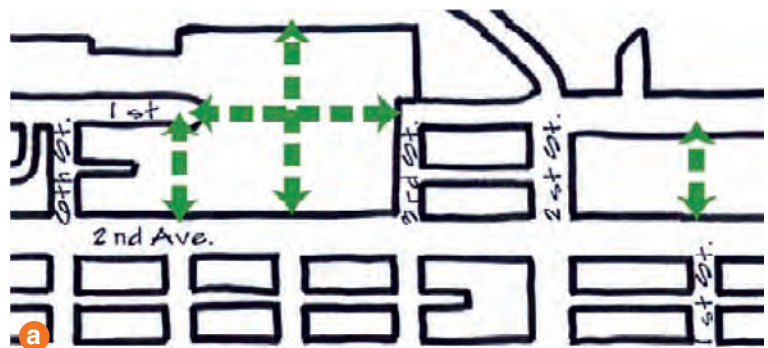
- » To encourage large sites to incorporate streets and pathways that connect to the surrounding street pattern of small blocks established by the original Brett and Hall town plan.
- » To demonstrate pedestrian focus at all scales, including site design, building orientation, building frontage and architectural details.

## Guidelines

- Subdivide large blocks with new streets that connect with the established fine grained downtown street network.
- Avoid and/or remove cul-de-sacs and other physical barriers which prevent people from walking or cycling through the downtown.
- Where large blocks are unavoidable, provide publicly accessible mid-block pedestrian and bicycle pathways.
- Increase pedestrian connections to the waterfront and surrounding residential neighbourhoods to create more options for pedestrians traveling to, from and through the City Core.
- Break up the visual mass of large buildings to reduce their visual impact from the pedestrian realm, and to create variation along the street. This can be achieved by incorporating minor visual breaks in their façades using upper storey step backs, changes in plane, and changes in material.
- The portions of buildings above 3 storeys in height should be a maximum of 60 metres in length along a street or public space. Upper portions of buildings that are longer than 60 metres in total length should incorporate a substantial setback such as a courtyard, public square, pedestrian walkway or other open space feature to break up the massing along the street edge.



Large blocks should be broken up to create smaller ones as re-development occurs.



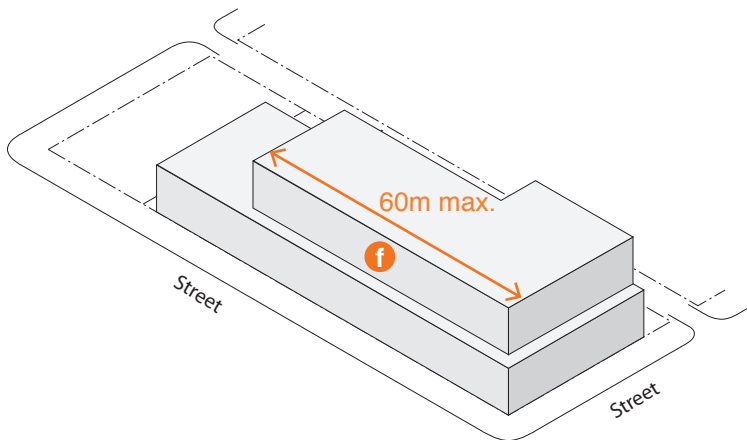
Large sites, such as the Rupert Square Mall and the Safeway site, should incorporate streets and pedestrian connections through the site on redevelopment, with buildings that front onto those connections and streets.



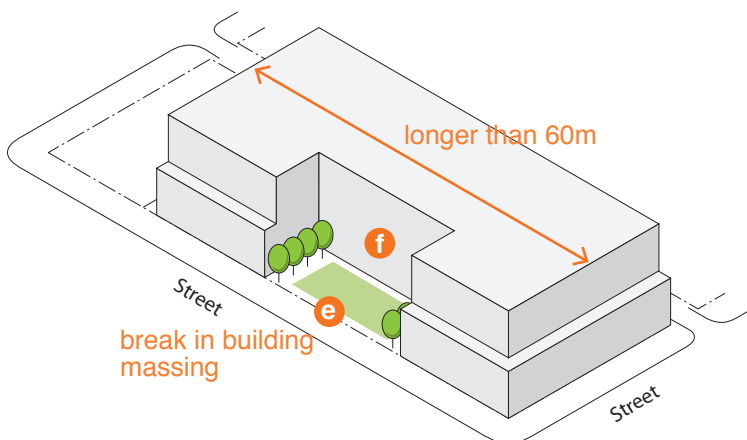
Pedestrian connections to, from, through and within the downtown should be enhanced and expanded.



A courtyard can be used to break up the visual mass of large buildings.



The length of buildings above the 3rd storey should be 60m in length (facing streets and public spaces) or less. If longer, see below.



Buildings taller than 3 storeys in height with over 60m in length above the 3rd storey should incorporate a substantial break in the building massing.

## Objective

- » To contribute to pedestrian scale, a sense of animation and the perception of quality through attention to detail.
- » To consider the three dimensional qualities of the façade, where architectural elements such as windows, doors and trim have depth, creating a dynamic play of light and shadow.
- » To promote articulation that indicates structural elements such as floor levels, interior spaces and even adjacent buildings.

## Guidelines

- a) The design of buildings should express a unified architectural concept that incorporates both consistency and variation in façade treatments by, for example, articulating facades into a series of intervals.
- b) Incorporate a range of architectural features and design details into building facades that are rich and varied in detail to create visual interest at a pedestrian scale. Examples include:
  - variation in height and massing
  - change in plane
  - datum lines, columns, pilasters, cornices, roof lines, sign bands, trim, molding
  - change of material
  - bay windows and balconies
  - decorative roof lines and cornices, stepped roofs
  - dormers and gables for sloped roofs
  - towers, cupolas, turrets, recesses and other architectural features
  - entrances
  - canopies and overhangs
  - ornament or integrated artwork
  - lighting.
- c) Design buildings to express their internal function and use.
- d) Buildings are generally intended to be built with minimal or no side yard setback and party walls with no windows. Over time, buildings will fill in to create a continuous street frontage. However, where there are blank side facades visible from street level, they should have a compatible architectural treatment such as:
  - wrapping the primary façade's architectural expression including materials and datum lines a minimum of 5 metres around the side
  - using a simplified but compatible materials palette across the entire blank wall
  - provision of public art or hand painted heritage-compatible advertising.



*Vertical and horizontal articulation, through changes in plane, reduce the perceived scale of large buildings.*



## Base-Middle-Top

e) Use step backs, projections, textures, detailing and materials to articulate a clearly defined organization of the façade that includes:

- Base: Within the first 3 storeys a base should be defined that contributes to the quality of the pedestrian environment through animation, transparency, articulation and material quality
- Middle: the middle or body of the building should contribute to the physical and visual quality of the overall streetscape. The design of the middle or body should consider the dimensions that are suited for its location and orientation on its site and in relationship to the base building
- Top: Clearly distinguish the upper storey, roof or roof line of the building's façade from its walls, for example, through the use of a cornice, projecting over hang, or decorative motif.



e

The expression of a base, middle and top is a traditional architectural technique used in Prince Rupert's historic buildings.



e

Facade articulation is equally applicable to traditional or contemporary buildings. Details can be simple.



# ARTICULATION

## Horizontal rhythm

- f) To reinforce a fine grain character of the street wall for all buildings greater than 7.6m in frontage, incorporate a rhythm of vertical elements in the organization of the façade, for example through columns, bays, datum lines, or window placement.



A rhythm of vertical elements in facades creates a fine grain.

## Windows and balconies

- g) Orient upper story windows and balconies to overlook adjoining public open spaces.
- h) Design balconies as integral parts of the building expression.
- i) In the Midtown and Downtown Districts, more traditional window proportions are desired. Refer to the District Guidelines.



Balconies are designed as integral parts of the building's composition through continuity of datum lines, bays, or materials.

## Roofline and rooftop mechanical

- j) Rooftop mechanical systems and penthouses should be integrated with the design of the building, contributing to the roofline expression, or, set back or concealed so that it is not visible from ground level.
- k) Ensure mechanical equipment, conduit, vents, flashing, telecommunications equipment and other rooftop systems are neatly detailed, as they can be visible from buildings and public spaces at a higher elevation.
- l) Green roofs are encouraged to provide aesthetic and sustainability benefits, and provide amenity space for building occupants.

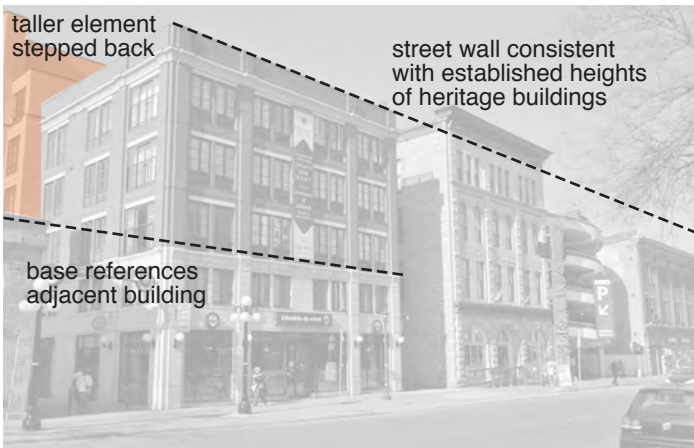
Screening of mechanical equipment is integrated with the building design and is part of the building's top.



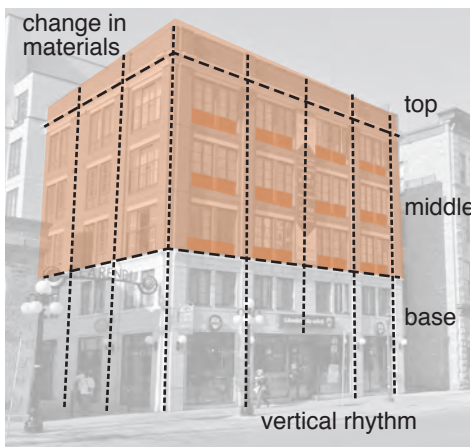
a b e f j



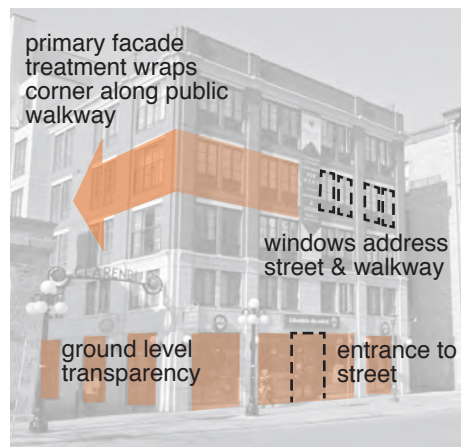
*This building is well articulated. It has a lower podium that relates to the street and its historic neighbours. It has an active facade with visual interest organized by vertical and horizontal datum lines creating rhythm.*



Massing



Datum lines



Relationship to public space

## Objective

- » To ensure buildings are sited and designed to create welcoming frontages and encourage street vitality, visual interest and safety.
- » To ensure entrances are visually and physically accessible from adjacent public spaces.

## Guidelines

- Provide active uses at ground level, with priority to retail, commercial or institutional uses that support high pedestrian activity.
- Where a building's program supports only a small quantity of retail, the retail space should be stretched out along the building's frontage at the street edge.
- Ground floor ceiling heights should generally be a minimum of 4.5m, to permit flexibility over time to adapt the ground floor to different tenants and uses.
- All uses should promote an animated street environment with frequent doors, windows, and pedestrian generating uses fronting directly onto the street.
- Ensure shop fronts are transparent. Transparency refers to where public and private realms meet. Good transparency is when interior (private) uses are visible from, and can even spill out onto, the sidewalk, and further, when public space is visible from inside buildings to allow for casual surveillance.
- Retail display windows should be located as close to the front of the façade as possible. Avoid deep columns, recesses or large building projections that reduce the visibility of retail windows as viewed from the sidewalk.



*Transparent store fronts creative activity, visual interest, and safety for pedestrians.*



When unavoidable, blank walls should be screened with landscaping or incorporate a patio cafe or special materials to make it visually interesting.



THIS



NOT THIS



A retaining wall can be used to make up grade differential across a site where it is well landscaped with hard and soft elements, and the primary building articulation still addresses the street.

## Blank Walls

g) Avoid expansive blank walls over 3 metres in length and retaining walls adjacent to public streets. When unavoidable, they should be mitigated using design treatments which can include, but are not limited to, the following:

- Providing art (such as a mosaic, mural, relief, etc.) over a substantial portion of the wall surface.
- Employing high quality materials consistent with the primary building expression, preferably stone or masonry.
- Providing special lighting, a canopy, awning, horizontal trellis or other pedestrian oriented features that break up the size of the wall's surface and add visual interest.
- Incorporating them into a patio or sidewalk café.
- Installing a vertical trellis in front of the wall with climbing vines or other plant materials.
- Setting the wall back to provide room for evergreen and conifers to provide year round screening.
- Terracing (stepping down) a retaining wall.

## Entrances

- a) Locate and design entrances to be prominent in the façade and create building identity. Modest exceptions to height, setback and step back are appropriate to emphasize entrances in the overall massing of the building.
- b) Use a high level of architectural detail and, where appropriate, landscape treatment, to emphasize primary entrances.
- c) Ensure main entrances to public buildings, offices, and residential lobbies incorporate weather protection through recess or overhang.
- d) Ensure all building entrances and transitions from outside to inside are barrier free and accessible through smooth grading of surfaces.
- e) For sites at street intersections, an entrance at the corner is encouraged, to reinforce the existing, unique design language of Prince Rupert's core area.
- f) Commercial entries tend to be public, and residential entries tend to be private. Accordingly, residential entrances should be architecturally differentiated from business entrances in mixed-use buildings.
- g) Provide pedestrian access to storefronts and businesses from the adjacent public street or space.
- h) Incorporate frequent entrances into commercial frontages facing the street with a maximum spacing of 15.2 metres.
- i) Recess commercial entrances to buildings from the sidewalk or property line a minimum of 1.2m in order to provide for door swings and to protect the entrance from rain or snow. Angled entry ways are strongly encouraged to help shield from wind. It reinforces the existing, unique design language of Prince Rupert's core area.



*Building entrance is prominent in the façade through techniques including modest increase to street wall height, change in materials and fenestration at corner, roof line overhang element, ground level canopy providing weather protection, and corner access.*



*Frequent entrances along commercial frontages can include both retail and apartment entrances.*



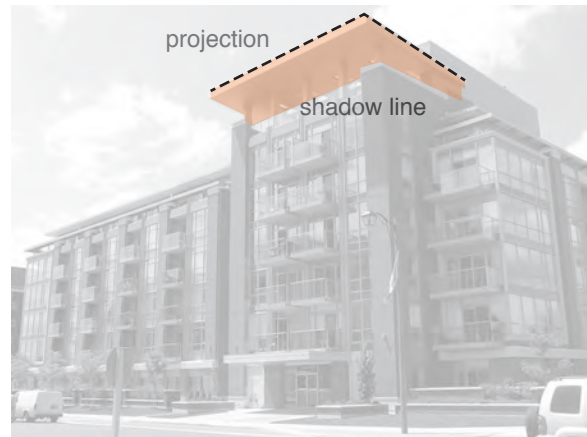
*Recessed entrances create punctuation in the street and protection from wind and rain.*

**b**

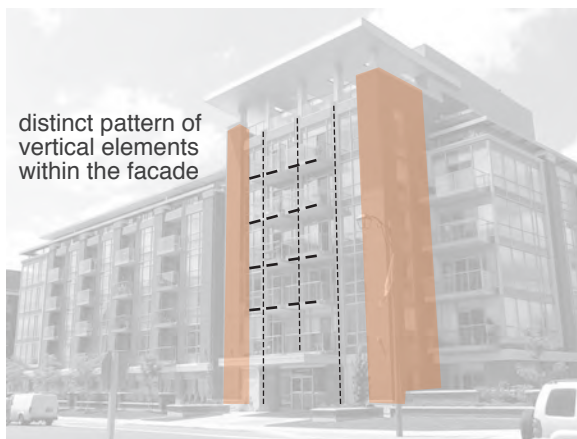
*Building entrances can be emphasized in a variety of ways. This well-articulated building uses the following techniques to make the entrance a prominent feature and create interest in the facade.*



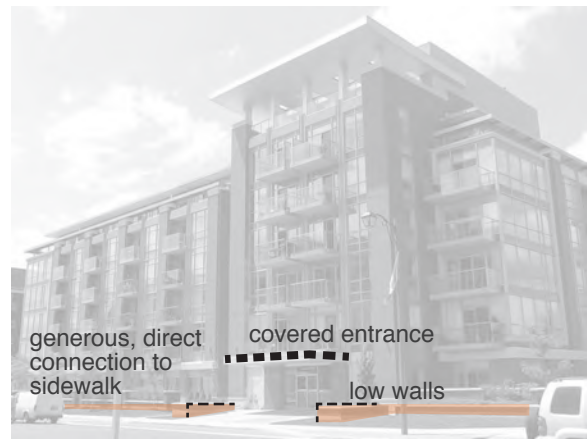
**Massing**



**Roofline emphasis**



**Articulation**



**Pedestrian emphasis**

## Objective

- » To ensure signs play a significant role in the character and animation of commercial areas.

## Guidelines

- Provide building address signage at all entrances.
- Signage should be designed, oriented and scaled to the pedestrian rather than the motorist, generally for viewing from ground level.
- Where there is retail at ground level, provide a sign band with a maximum height of 1.5m within the façade design at the first storey.
- Integrate signage into the organization and design of building facades by placing them within sign bands, architectural bays, friezes and datum lines.
- Signage should not obscure windows, cornices or other architectural elements.
- Retail-commercial signage should be in scale with the building or storefront.



Street addressing.



Pedestrian scaled signs provide interest and animation of the public realm.

## Encouraged

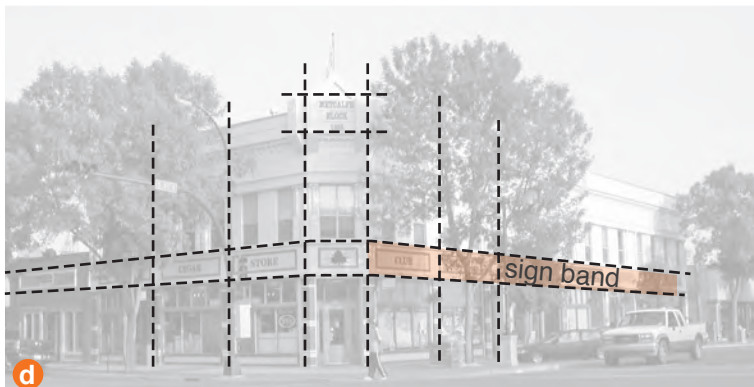
- Projecting two-dimensional or “blade” type signs mounted on buildings, or suspended underneath canopies
- Frames, graphics and letters that incorporate three dimensionality and low relief
- Individual cut-out or silhouette letter signs, with or without illumination.
- Externally lit signs
- Vertical signs or banners
- Neon signs
- Cut out letter and graphic signs applied directly to windows occupying a maximum of 25% of the window’s surface area
- Signs applied to an awning’s surface material. They should be modest in scale, occupying a maximum of 25% of the awning’s surface area and with a maximum letter height of 50cm.
- Sandwich board signs on the sidewalk. They should have a maximum height of 1.2m and should never be placed to obstruct a minimum 2.0m pedestrian clearway along the sidewalk.

## Acceptable

- Flush mounted fascia signs

## Discouraged

- Large signs on the surface of awnings
- Signs mounted above canopies
- Internally lit plastic box signs with flush surfaces
- Pylon (stand alone) signs
- Rooftop signs



**d** Signs are organized into the datum lines of the facade. A traditional sign band is located above the retail display windows.



**Encouraged:** blade signs hanging from weather protection or mounted to the building.



**Encouraged:** three dimensional lettering; sign bands; modest window and awning signs.



**Discouraged:** internally lit and plastic box signs; signs mounted on buildings or weather protection without regard for datum lines, especially when large.

## Objective

- » To ensure building lighting enhances the overall quality and character of the City Core.
- » To ensure lighting is fully integrated within site and building designs.
- » To make a positive contribution to the sense of safety and security of pedestrians.
- » To provide supplementary lighting to street lighting.

## Guidelines

- All lighting should be night sky compatible and bird friendly.
- Illuminate storefronts, decorative building facades and architectural features by providing lighting on the face or interior of buildings.
- Illuminate retail display windows from within the building.
- Entry areas and walkways on private property should be well lit to promote pedestrian comfort and security.
- Full-spectrum “white” light is preferred in public areas.
- Ensure lighting is sensitive to nearby residential uses. Avoid visible, glaring light sources by using down-and/or up-lights with cut off shields.
- Gooseneck lights and sconces applied to fascias are preferred in commercial areas.
- Incorporate valence lighting into canopies to illuminate sidewalks and walkways.



Building lighting is complementary to street lighting, and serves a decorative function to highlight architectural features, signs, and display windows.



A combination of wall mounted lights, valence and surface lighting animate this building facade and adjacent pedestrian areas.



Canopy lighting (left) and gooseneck lighting (right).



*Sensitive incorporation of new weather protection minimizes obscuring of the building facade and historic detailing.*



*Weather protection organized in modules and terraced to step down a slope.*

## Objective

- » To shelter pedestrians from rain, wind and sun along sidewalks in pedestrian-oriented shopping areas and public spaces.
- » Figure 2: Weather Protection identifies priority areas for weather protection.

## Guidelines

- Provide weather protection over store fronts and display windows for pedestrian comfort, using overhangs, awnings, canopies or arcades.
- Provide weather protection for primary building entrances to curbside taxi zones or drop-off areas in front of theatres and places of entertainment, hotels, restaurants, and major public buildings.
- Provide weather protection on buildings at street corners (for the benefit of people waiting for traffic lights) and adjacent to bus stops.
- Awnings, canopies and overhangs should be architecturally integrated into the design of the façade, and incorporate design features of the buildings from which they are supported. This includes consideration of the building façade's datum lines, fenestration pattern (placement of windows and doors), style, scale, form, and historic period.
- Place awnings and canopies on building facades in a manner that does not obscure the building façade or historic detailing.
- Where sidewalks slope, organize awnings and canopies in modules to follow the general profile of the sidewalk slope.
- Avoid continuous opaque (solid) weather protection along the full length of facades. Provide gaps or transparent sections to allow some light penetration to the sidewalk.

# WEATHER PROTECTION

Weather protection is encouraged throughout the City Core, with key areas where it is a priority, as identified below.

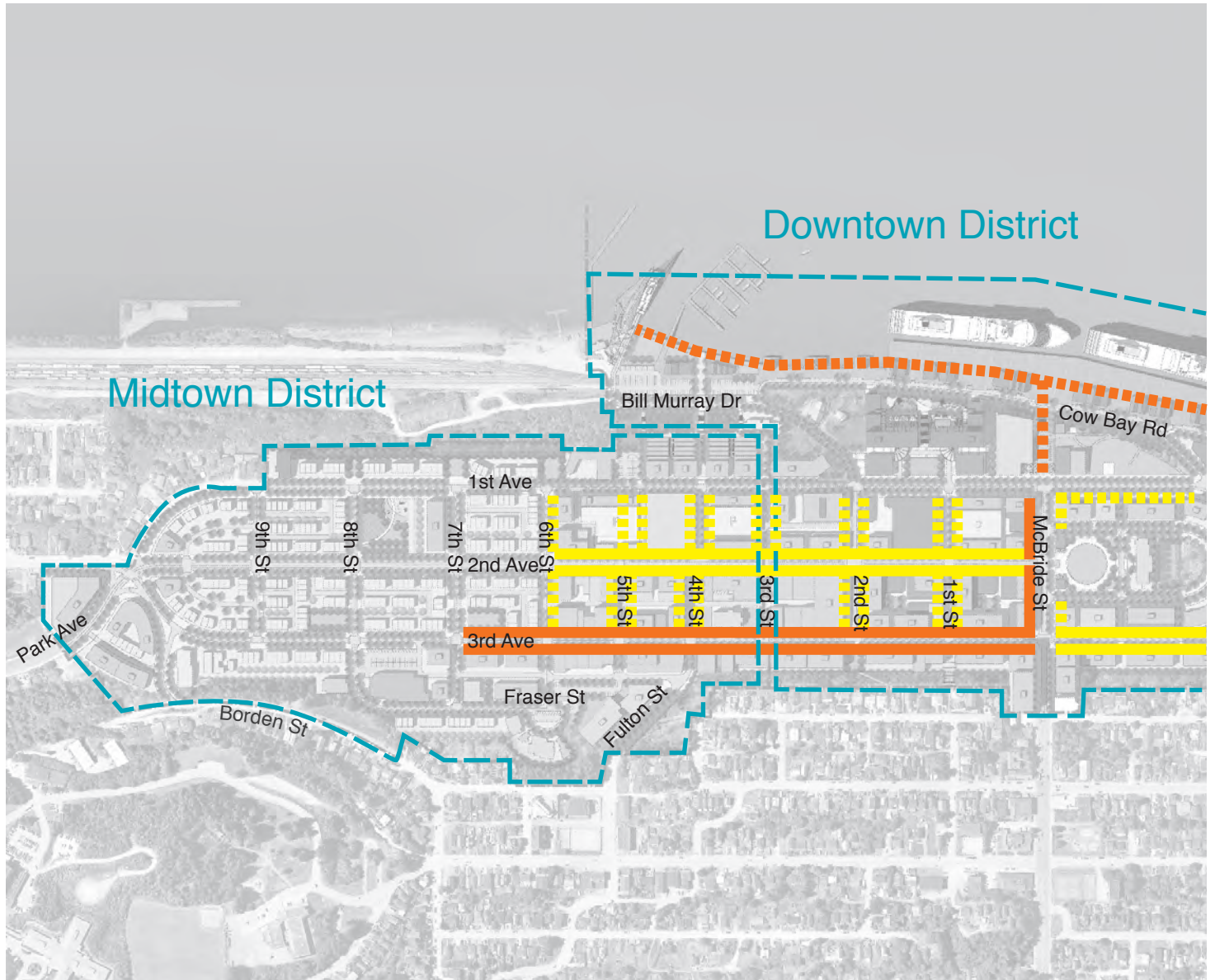
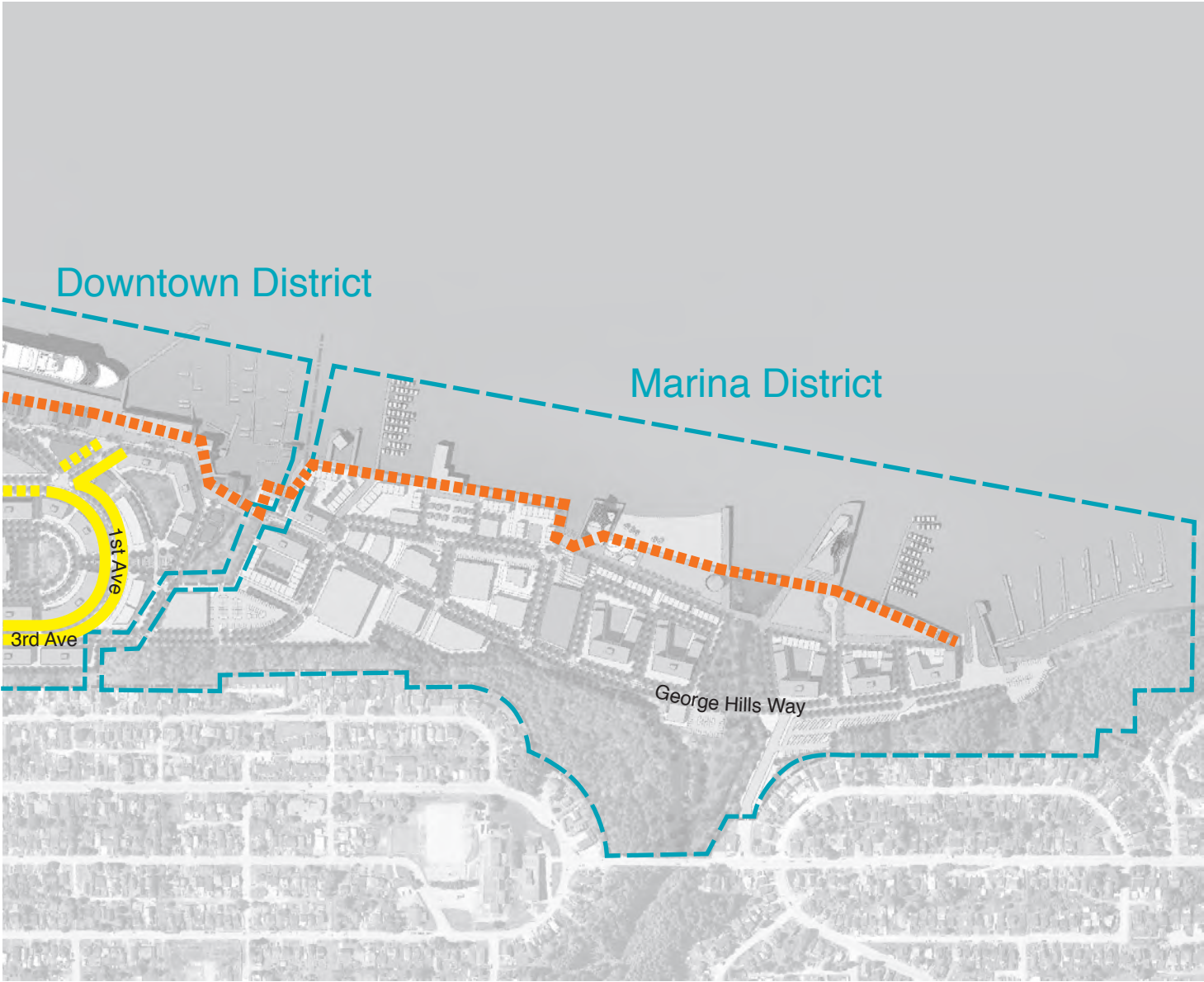


Figure 2: Priority Areas for Weather Protection



**Priority Area**

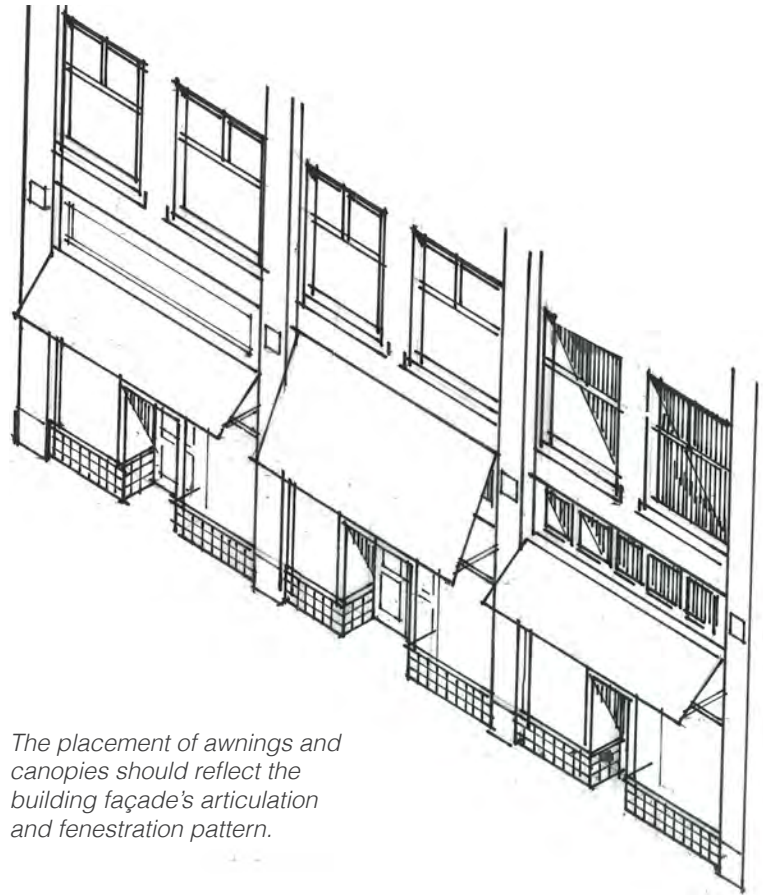
- Continuous weather protection
- Intermittent weather protection

**Encouraged**

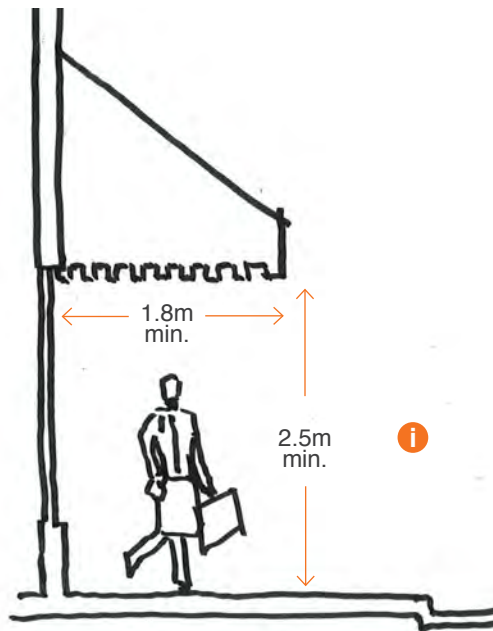
- Continuous weather protection
- Intermittent weather protection

## Awnings

- h) Awnings are preferred over canopies for use on storefronts less than 10m in width and in areas where there is an established eclectic character resulting from a variety of existing awnings, such as along 3rd Street.
- i) Awnings should have a minimum vertical clearance of 2.5 m measured from the sidewalk and should extend out over the sidewalk a minimum of 1.8 m, with greater coverage desirable in areas of high pedestrian traffic and where sidewalk widths are adequate. Awnings should not occupy more than 2/3 of the total sidewalk width.
- j) Awnings should have a minimum slope of 30 degrees, and should be tightly stretched over a rigid metal frame, to allow for proper drainage and self cleaning action of rain and wind.
- k) 3 or 4-point awnings are preferred.
- l) Construct awnings of durable, colour-fast material. This may include reinforced plastic coated fabric provided the look and feel of canvas is maintained.
- m) Avoid the use of quarter barrel awnings.

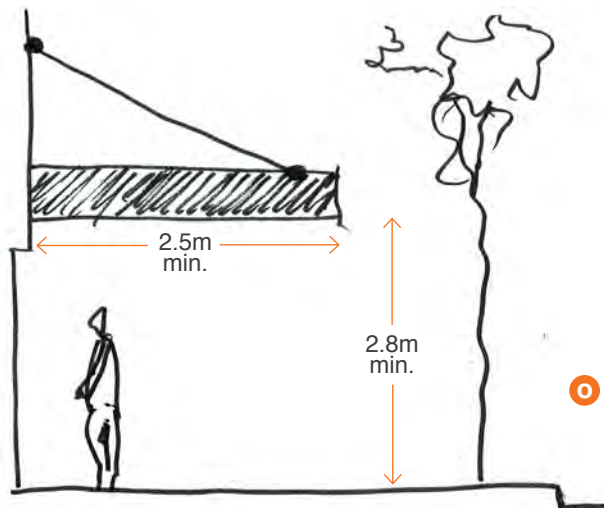


*The placement of awnings and canopies should reflect the building façade's articulation and fenestration pattern.*





Transparent canopies made of glass and steel, or those incorporating transparent bands or panels, are preferred over solid, opaque canopies.



## Canopies

- n) Canopies are preferred for use on building frontages over 15 m, along sloped sidewalks, along major pedestrian routes having a predominance of existing canopies, and on theatres and other buildings in front of which significant waiting occurs.
- o) Canopies should have a minimum vertical clearance of 2.8 m measured from the sidewalk and should preferably extend out over the sidewalk at least 2.5 m while maintaining a minimum 1.2m setback from the outer face of the curb.
- p) Canopies should be supported entirely from the building. Post supports for canopies within city property are not permitted.
- q) Use transparent and translucent canopies to allow natural light to penetrate to storefronts and the sidewalk.
- r) Steel and glass are the preferred materials for canopies. Materials should be non-combustible.

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## Objective

- » To use durable, high quality materials that are appropriate for Prince Rupert's climate.
- 
- » To ensure the quality of materials and their detailing fosters a sense of character and timelessness.
- 

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## Guidelines

- a) Choose materials for their functional and aesthetic characteristics to exhibit quality of workmanship, longevity, sustainability and ease of maintenance.
- b) Use materials and fastening systems that are authentic to their purpose and neatly detailed. Do not use materials that imitate other materials.
- c) For traditional building styles, choose materials and architectural details such as cornices, sign bands, lintels, etc. that are consistent with the chosen architectural style.
- d) For contemporary building styles, ensure materials are crisply detailed with consistent reveals. Inexpensive materials, in particular, must be used creatively and exhibit a high quality of application and fastening.
- e) Changes of material should be purposeful and coincide with substantial massing elements or datum lines of the building. Generally, changes of material should not occur at building corners; a material return is preferred.
- f) To create visual interest, consider using a dominant and 1-2 subordinate materials for the primary facade, in addition to glass and window surround materials for windows.

## Encouraged

- Masonry: stone and brick
- Glazed tile
- Concrete: cast in place, precast, flat profile tiles
- Pre-finished, non-corrugated metal used consistently in either horizontal or vertical arrangements
- Corrugated metal, marine grade painted steel, weathering (corrosion-resistant) steel
- Clear glass, including low iron glass (ultra clear) for shop windows
- Standing seam metal roofing for sloped roofs

## Acceptable

- Marine grade stained or painted wood
- Hardboard with integral colour
- Vinyl for window frames

## Discouraged

- Tinted, figured or mirror glass that obscures transparency, especially at ground level
- Vinyl siding
- Stucco
- Plastic



Corrugated metal is used creatively as a reference to shipping containers. It is crisply detailed, and transitions neatly to other materials such as glass.



**Preferred:** brick, steel, clear glass



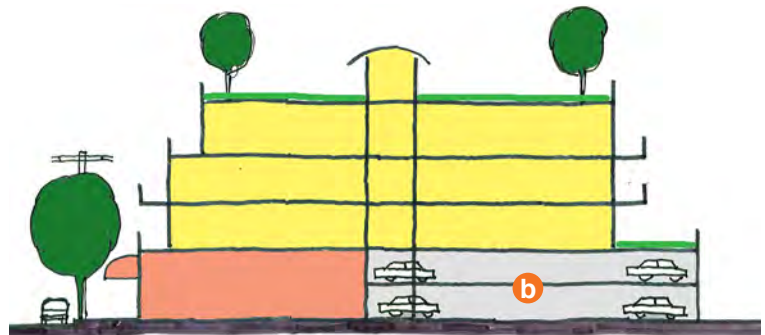
The interplay of materials helps to activate this facade and act as datum lines defining zones.

## Objective

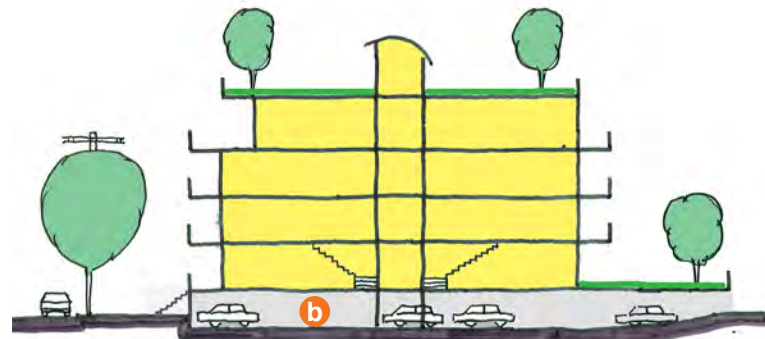
- » To minimize the physical and visual impacts of servicing, vehicle access and parking on safety and attractiveness of the public realm, while recognizing the importance of these functions to the success of the City Core.

## Guidelines

- A welcoming pedestrian environment with continuous street edge definition and active use is critical to the character of the City Core, particularly along retail streets. Therefore, it is important that vehicular and service functions and other back of house activities remain primarily accessed from rear lanes wherever possible, so as not to conflict with pedestrian oriented street activity.
- Structured underground or tuck-under parking is preferred over off-street surface parking.
- Where off-street surface parking is unavoidable, it should be located to the rear of the building with parking access from a lane or side street.
- Off-street parking should not be located between a building and the public sidewalk, nor within individual garages that face onto streets or public spaces.
- If located beside the building and adjacent to the public sidewalk, screen surface parking areas from sidewalks and public spaces using materials that provide a visual buffer while still allowing clear visibility into the parking areas from adjacent sidewalks and building entrances to promote personal safety and security. Screening could include landscaping, low screen walls, decorative fencing, a trellis, or grillwork.



Structured parking that is fully above ground level should be located at the rear of sites, with active ground floor uses between the parking and the street or public space.



Structured parking that is partially above ground level should be completely concealed by the base of the building, generally no more than 1 metre above grade.



Surface parking adjacent to the sidewalk screened by hard and soft landscape.

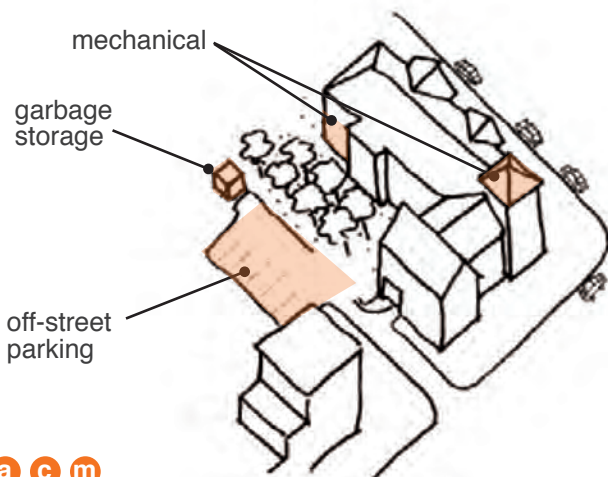
NOT THIS



THIS



Off-street parking located behind, underneath or beside, but never between the front of a building and the public sidewalk.



**a c m**

Off-street parking and servicing located to minimize impacts on pedestrian realm.

f) In general, vehicular access should be from a lane or side street. Where there is no lane, and where the re-introduction of a lane is difficult or not possible, access may be provided from the street, provided:

- There is minimal interruption of the pedestrian realm and streetscape treatment.
- Waiting, or pick-up drop-off areas are located internal to the site, not in the public right-of-way.
- There is no more than one interruption per block face and only one curb cut on the street.
- Design clearly prioritizes pedestrian movement.

g) Avoid ramps accessed directly from a street. Minimize negative impacts of parking ramps and entrances through treatment such as enclosure, screening, high quality finishes, sensitive lighting, and landscaping.

h) Any vehicular entrance to a building from the street, including its associated components (doorways, ramps, etc.) should be architecturally integrated into the building design.

i) Clear lines of sight should be provided at access points to parking, site servicing, and utility areas to enable casual surveillance and safety. They should be well lit at night.

j) Shared parking and access is encouraged where possible.

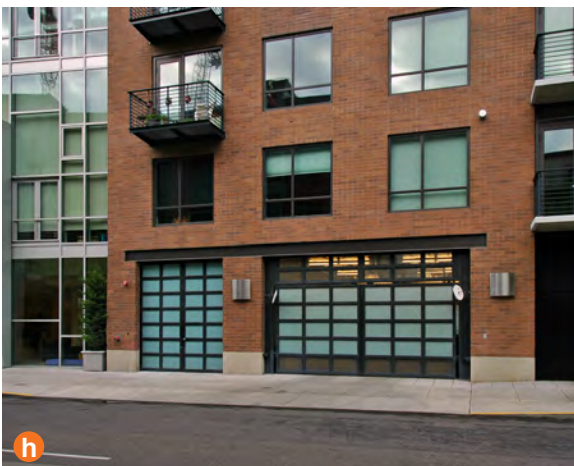
k) Large parking lots should be avoided and should be broken into smaller ones through the use of walkways, lighting and low landscaping. Continuous parking areas are limited to 50 stalls without the use of a break.

# SITE SERVICING

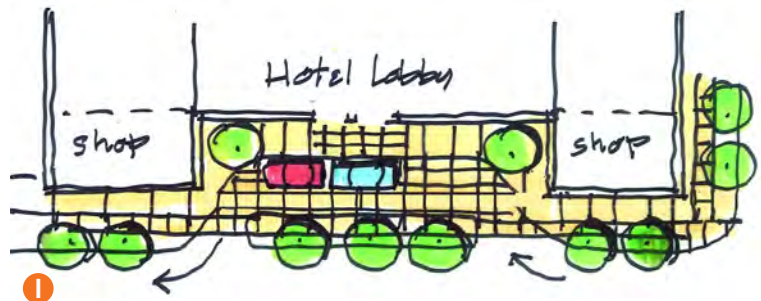
- l) Incorporate adequate and comfortable drop-off areas for hotels that are directly adjacent to lobbies and that minimize impacts on the pedestrian realm and streetscape.
- m) Locate utility meters, service meters, vents, telecommunications gear and other necessary mechanical equipment discretely:
  - Preferred locations are along rear laneways
  - Where they are visible from public spaces, they should be integrated into the design of the building through techniques such as recesses, enclosures, and under steps or porches
  - Screened by landscaping
  - Or, use enhanced materials or public art such as a mural treatment.
- n) Integrate garbage storage areas into the building design wherever possible.



*Large parking lots subdivided into smaller units with walkways and landscaping.*



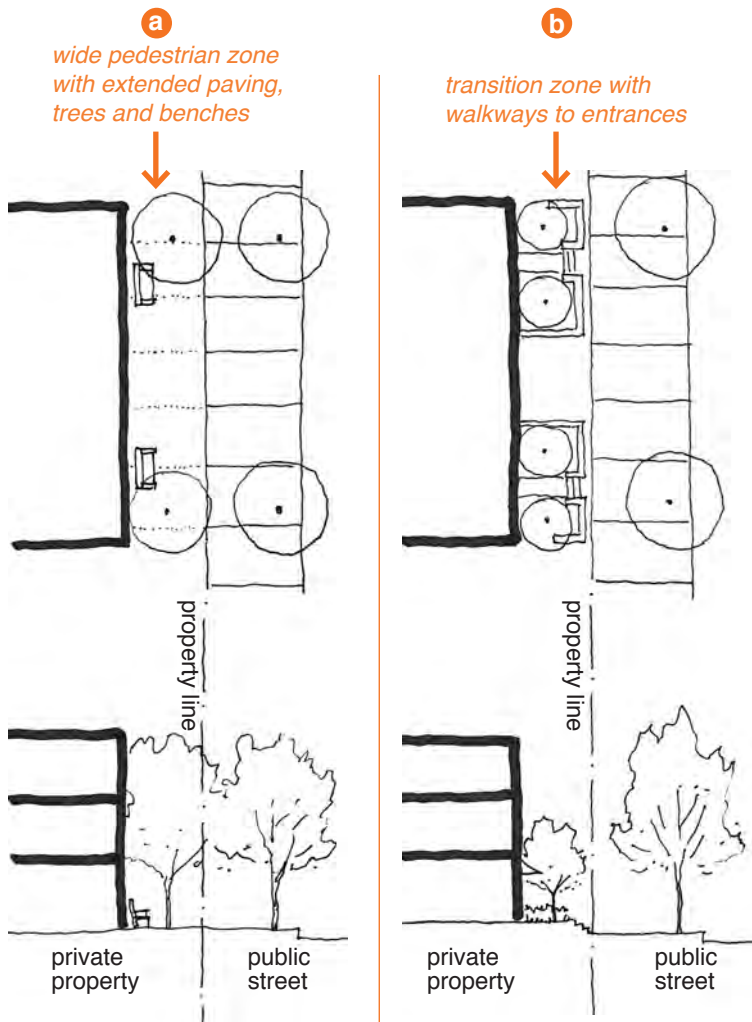
*Loading doors, parking doors and service rooms integrated with the design of the facade.*



*Hotels should incorporate convenient and safe drop-off areas directly adjacent to lobbies while minimizing impacts on the pedestrian realm.*

# LANDSCAPING

*These guidelines are intended for street edge landscaping, not for plazas, squares, parks, courtyards or mid-block connections.*



*Where ground level uses are active, such as retail or commercial, private landscaping becomes an extension of the public sidewalk.*



*Where ground level uses are less active, such as residential, private landscaping addresses the street edge, creates a front yard zone, and provides a transition.*



## Objective

- » To reinforce streetscapes by supplementing, where necessary, the primary role of the street wall in defining the street edge.
- » To provide amenity for pedestrians and building occupants.
- » To reinforce an 'urban' landscape appropriate to the City Centre and avoid a suburban character.
- » To establish well designed zones from public, to semi-public, to semi-private and private.

## Guidelines

- Where a building is set back from the street edge, a widened public realm sidewalk is preferred, with, where space permits, additional street trees, and pedestrian amenities such as seating, lighting and public art.
- Where a building is set back from the street edge but ground level uses do not support a widened sidewalk, a transition should be created between the sidewalk and the building face through techniques such as grade separation, low walls or fencing, planting, and walkway connections to building entrances. This creates a semi-private front yard amenity space for residents.
- Trees on private property should reinforce the primary tree planting on the public street in terms of species selection, location, spacing and planting condition. Where there are no street trees, trees on private property should be located to reinforce the façade articulation of the adjacent building.
- Low landscaping should reinforce the street edge and walkway connections.

## Objective

- » To celebrate and enhance Prince Rupert's character defining elements such as its topography, rock outcrops, significant stands of trees, shoreline, and harbour area.

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- » To celebrate Prince Rupert's existing cultural and built heritage.

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- » To ensure buildings in visually prominent and landmark locations express a higher design standard.

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- » Figure 3: Visually Prominent Sites identifies special sites in the City Core where the following guidelines apply.

---

## Guidelines

- a) Locate and design buildings to respond to specific site conditions and opportunities including irregularly shaped lots, location on prominent intersections, corner lots, unusual topography, significant vegetation, views and other natural features.
- b) New development and landscaping should frame rather than block public views of prominent natural features, landmark sites and buildings, public art and other prominent downtown features.
- c) Street end views of the harbour should be restored through redevelopment of large sites such as Rupert Square Mall and in the Marina District as opportunities arise.
- d) On corner sites, develop both street facing facades as front elevations with pronounced entrances oriented to the corner and/or primary streets.
- e) Minimize impacts from sloping sites on neighbouring development, for example, by using terraced retaining walls of natural materials or by stepping a project to respond to the slope.
- f) Taller building elements at gateways and landmark sites/frontages are encouraged, such as towers, rotundas, porticos, change in building plane, overhangs, special rooflines, public art, and street wall height exceptions, where those elements exhibit:
  - Compatibility with adjacent context, including appropriate scale
  - Compatibility with the principal building expression
  - Design excellence.
- g) Enhanced architectural features are encouraged at corners, including corner entrances.
- h) Enhanced quality of materials and detailing is encouraged.
- i) Where key views from a higher elevation overlook the buildings below them, ensure the roofscapes of the lower buildings are designed with enhanced architectural treatments, and all mechanical equipment is screened and neatly organized.
- j) Surface parking visible from the street edge is not appropriate at gateway locations.



Both frontages of corner sites should be articulated as primary facades, with a primary entrance located at the corner. Architectural elements respond to corner and gateway sites.



Street ends should be maintained as open space to preserve views of the harbour. As development occurs over time, new buildings should reinforce the view corridor. Important view corridors include those to the harbour and to landmark sites.

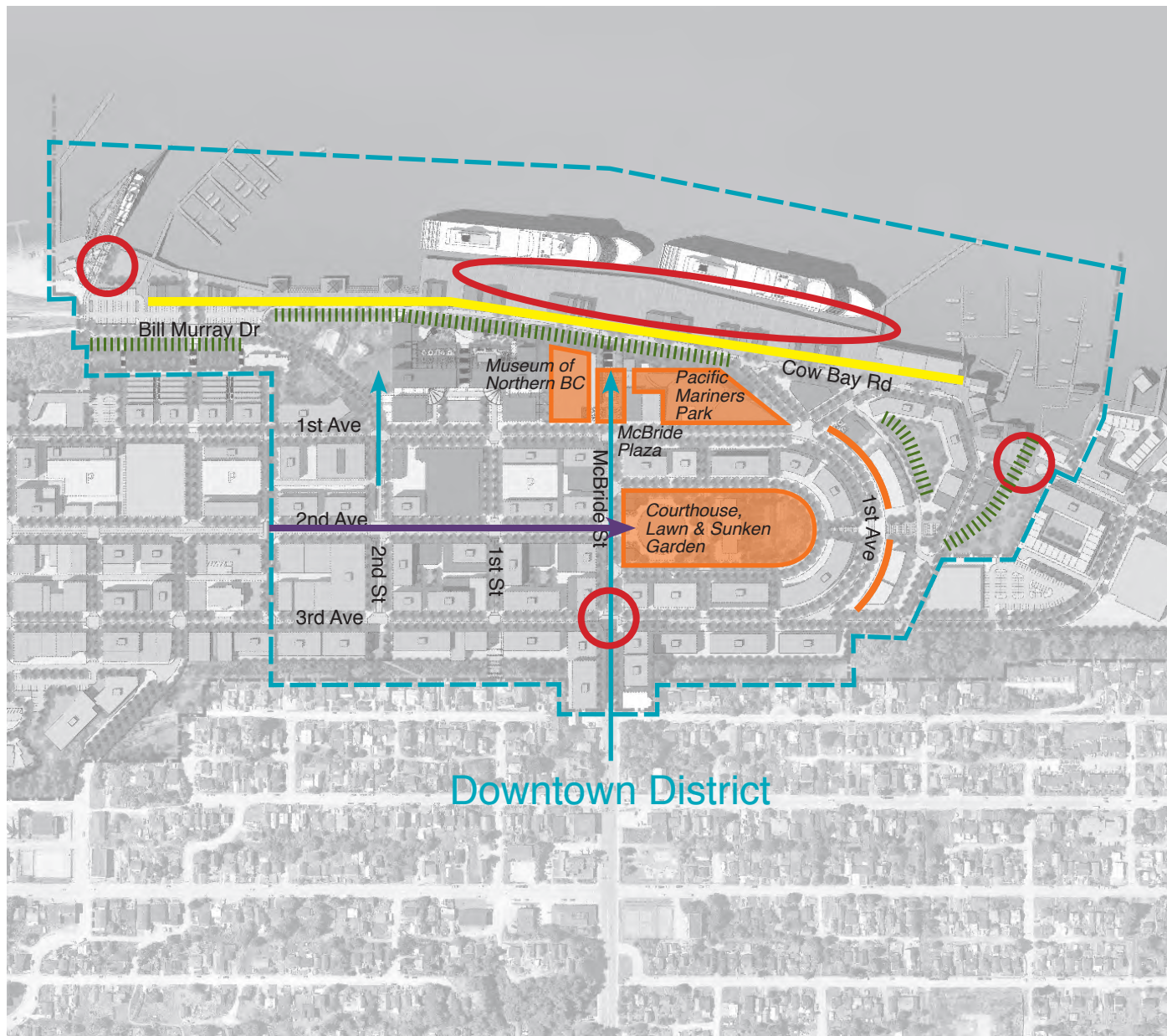


Enhanced building elements are encouraged at corner, gateway and landmark sites.

# VISUAL PROMINENCE



Figure 3: Visually Prominent Sites



- Landmark site
- Landmark frontage
- Prominent natural feature
- Gateway
- View corridor to harbour
- View corridor to landmark site
- Prominent roofscape

# VISUAL PROMINENCE

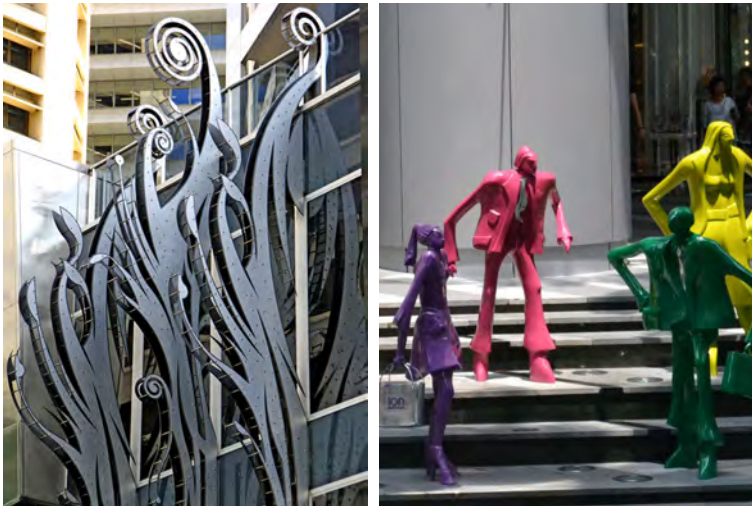


Figure 3: Visually Prominent Sites

- Landmark site
- Landmark frontage
- Prominent natural feature
- Gateway
- View corridor to harbour
- View corridor to landmark site
- Prominent roofscape



Encouraging more murals in Prince Rupert will enliven the public realm and provides opportunities to showcase local culture.



Public art can be integrated with the building's structure and expression (above), or be stand-alone (top).

## Objective

- » To enhance the experience of the public realm with high quality public art.
- » To ensure public art opportunities are consistent with the city building objectives of the City of Prince Rupert.
- » To tell the story of the place and peoples of Prince Rupert

## Guidelines

- a) Priority locations for public art are visually prominent locations such as gateways, corners, landmark sites, and important view corridors. Public art should be clearly visible and physically accessible to the public.
- b) Public art should enhance the public realm through artistic excellence and originality, and be appropriate to the site or location's physical and cultural context.
- c) Public art should not obstruct pedestrian, cyclist or vehicular circulation, entrances, windows, or sight lines to important natural and built features.
- d) Public art should not impact, or be diminished by, existing utility locations.
- e) Public art should exhibit high quality construction, installation and materials, as appropriate for its intent.
- f) Appropriate maintenance procedures should be secured. Public art should not obstruct maintenance of the public or private realm.
- g) Selection of public art should include the involvement of the City of Prince Rupert's cultural representatives (e.g. a cultural development officer, a public art advisory committee, or a public art jury) to ensure consistency with the goals of the OCP, these City Core Development Permit Guidelines for Commercial, Industrial and Multifamily Development, and any relevant City policies.

## Objective

- » To ensure universal access to buildings and sites.
- » To promote building siting, orientation, and design that enhances feelings of personal safety and security.

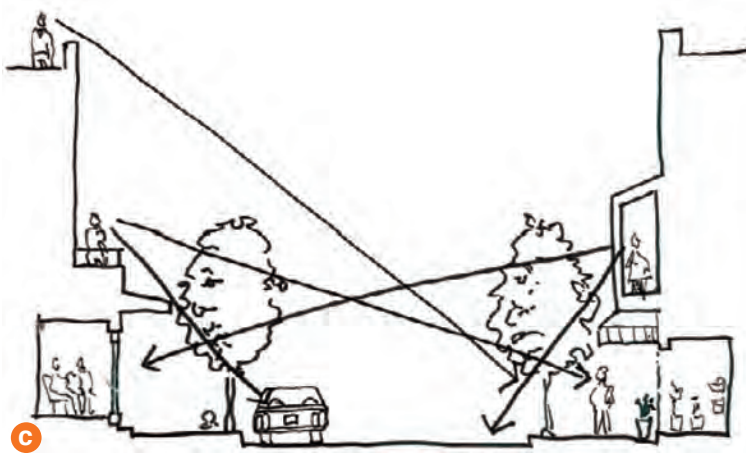
## Guidelines

### Accessibility

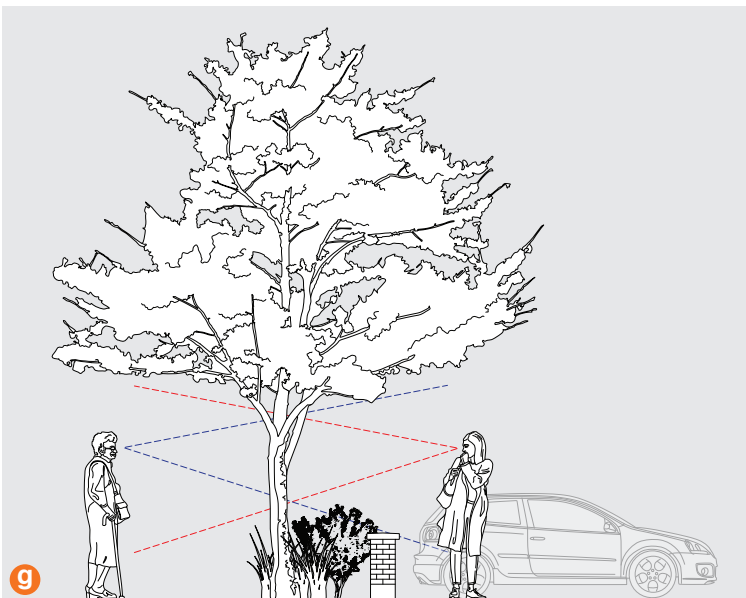
- Ensure pedestrian routes including those leading to building entrances are safe and easy to use by all people, including those using mobility devices and guide animals. Routes should be direct, level, obstacle free, easily identifiable and clearly separated from vehicular routes.
- Provide accessible options for site furnishings, where present, including seating and waste/recycling bins. Accessible seating should include armrests for assistance, and clear areas in front and to one side for people using mobility devices.



**b** Accessible seating.



*Buildings should be designed and oriented to encourage casual surveillance and 'eyes on the street.'*



*An open field of view at eye level promotes observation and safety.*

## Safety

- c) Ensure the design of new development, through the placement of street-level uses, entrances, windows and balconies contributes to “eyes on the street” and allows for casual surveillance of streets, parks, open spaces, and children’s play areas.
- d) Avoid blank, windowless walls that do not permit people to observe the street from inside buildings.
- e) Provide lighting at all common entrances, in parking areas, along all internal walkways, and in laneways.
- f) If necessary for security purposes, security measures such as grilles over ground floor windows, or fencing, should be ornamental and complement the architectural expression.
- g) Ensure structures, landscaping and plant materials maintain an open field of vision between 1.0m and 2.5m above ground level, and do not provide hiding places for undesirable activity. If elements such as front yard fencing is taller, ensure it is visually permeable such as wrought iron.

# RESIDENTIAL BUILDINGS

*These guidelines apply to buildings that only have residential uses, or wherever there are residential uses at ground level.*

## Objective

- » To ensure the ground floor of residential buildings have well articulated, active facades that address streets and public spaces.
- » To provide frequent entrances along the street to encourage activity.
- » To provide a semi-private transition zone between the public sidewalk and the residential building.



*Buildings should be designed and oriented to encourage casual surveillance and 'eyes on the street.' A defined semi-private zone acts as a transition to the street.*

## Guidelines

- a) Site and orient multi-family residential buildings to overlook public streets, parks, and walkways and private communal spaces while ensuring the security and privacy of its residents.
- b) Set back residential buildings on the ground floor a minimum of 1.5 metres to create a semi-private entry or transition zone to individual units, such as for a porch, stoop, landscape area, or elevated entryway.
- c) Apartment lobbies and main building entries should be clearly visible from the fronting street, and have direct sight lines into them. Where possible, apartment lobbies should have multiple access points to enhance building access and connectivity with adjacent open spaces.
- d) Where located adjacent to existing or planned commercial sites, design residential buildings to accommodate future changes to ground floor use by incorporating a minimum ground floor to ceiling height of 4.5 metres. This allows for adaptability and flexibility over time.
- e) Articulate individual units at ground level in the design of the facade and incorporate individual entrances to ground floor units in residential buildings that are accessible from the fronting street or public space. This provides easy pedestrian connections to buildings, encourages street use and walking and enhances safety.
- f) Emphasize entrances to individual units through porches, covered stoops, cornices, transoms, side lights, and building massing.



*Ground floor residential uses should step back to create a semi-private transition zone. Individual ground floor units with direct access from the sidewalk enhances street activity.*



*In the City Core, residential buildings should be located close to the sidewalk to provide street definition, intimacy and an urban character. A transition area is important to provide a sense of privacy and separation for ground floor units.*

## Why this building works

### Placement

- located close to the street edge, defines the sidewalk

### Ground Floor

- individual units have doors and windows facing the street
- a narrow semi-private landscape zone provides a transition

### Active Facade

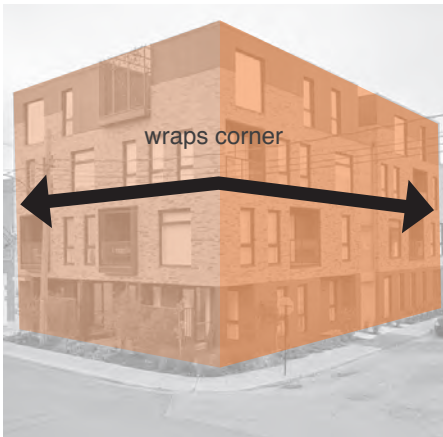
- windows and balconies overlook the street
- variation in massing, materials, colours, projections and window proportion add interest

### Context

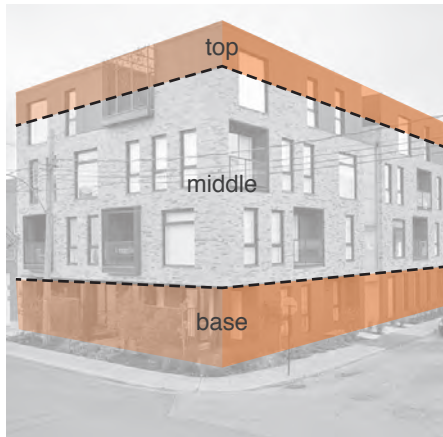
- building has a unique character and expression while reinforcing the urban context



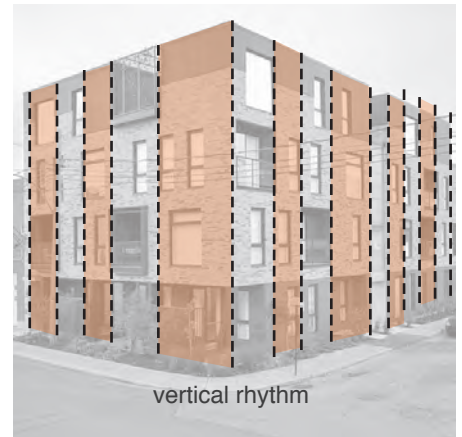
# RESIDENTIAL BUILDINGS



Wraps corner



Horizontal articulation



Vertical articulation



Ground level design



Massing articulation



Materials



Entrance

# OFFICE BUILDINGS

*These guidelines apply to buildings that only have office uses, or wherever there are office uses at ground level.*

## Objective

- » To ensure office buildings are articulated at a pedestrian scale, and contribute to the quality of the City Core on par with mixed use buildings.

## Guidelines

- Organize the ground floor of office buildings to present the most animated uses or functions to the street, such as lobbies, cafeterias, and gathering areas, with a highly transparent façade.
- Provide occupant signage (company names) in a similar fashion to retail signage. Locate signs above entrances or within a sign band above ground level. An upper level company sign is also permitted, provided it is located below the roofline.



## Why this building works

### Placement

- located close to the street edge, defines the sidewalk

### Ground Floor

- windows and multiple entrances provide animation along the sidewalk
- servicing entrance is integrated into the design of the facade and uses high quality materials

### Massing

- maintains a two storey street wall
- a long building frontage is articulated into a series of smaller volumes
- the massing creates a dynamic roof line

### Materials

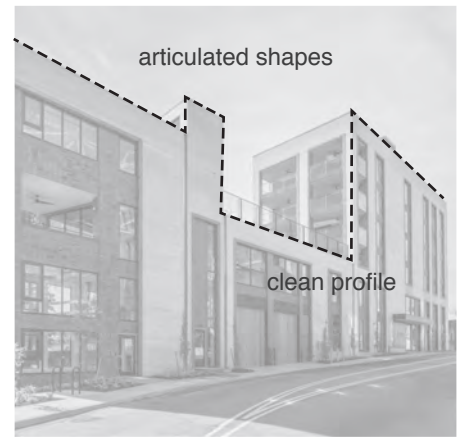
- metal, glass and two tones of brick are used in a variety of ways to create an active facade
- materials are used consistently across the building to unify a complex form



Horizontal articulation



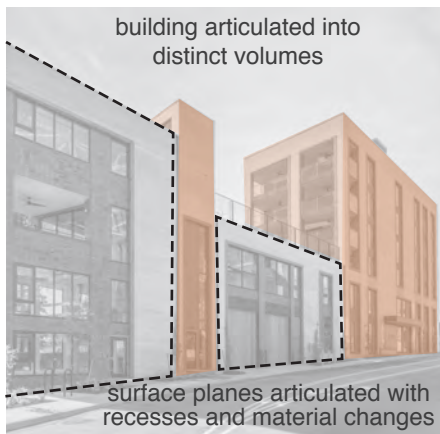
Vertical articulation



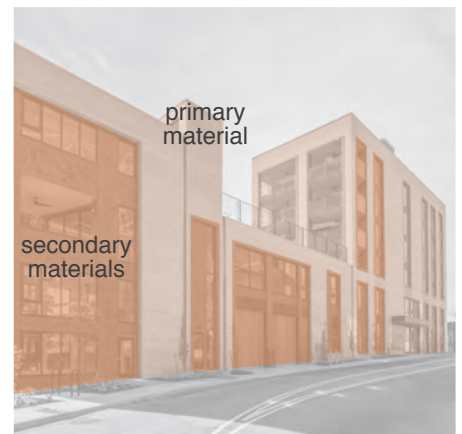
Roofline



Ground level design



Massing articulation



Materials



Servicing entrance



Landscaping

# LARGE RETAIL BUILDINGS

*These guidelines apply to large format retail stores in either mixed use or single use configurations.*

## Objective

- » To ensure large retail buildings or stores, where appropriate, maintain the pedestrian scale of City Centre streets.

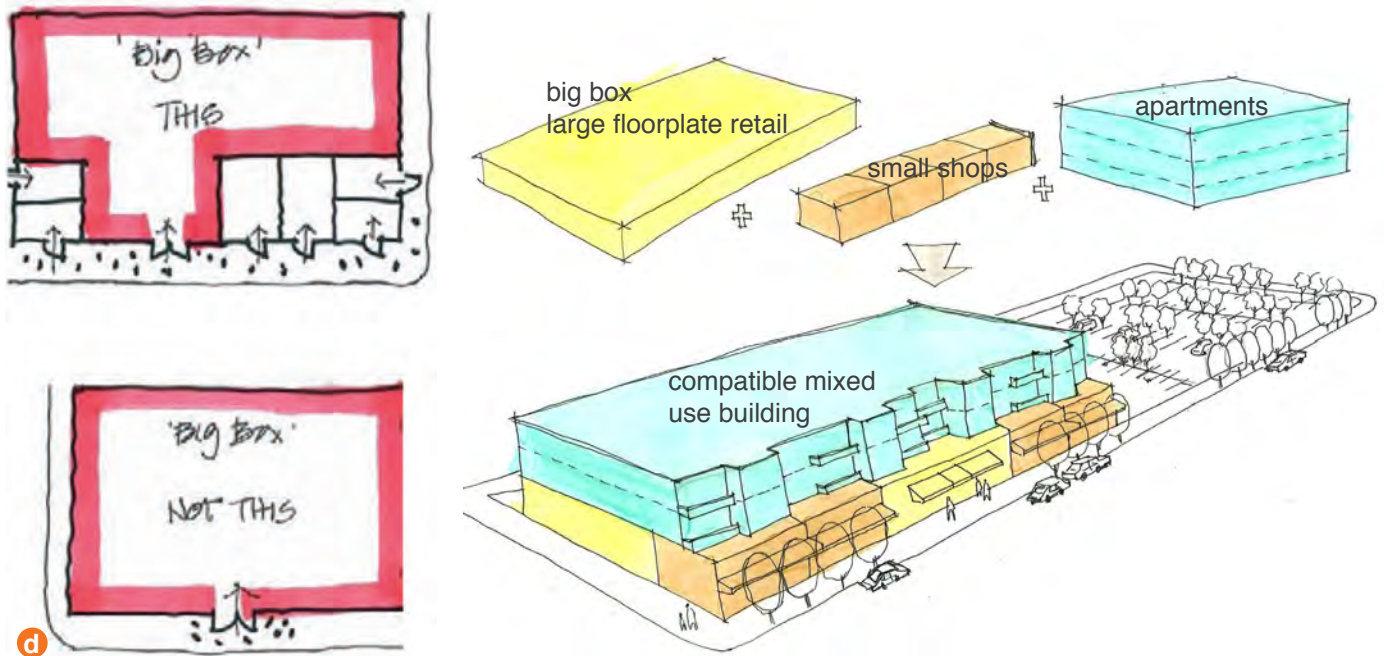


*Smaller sized stores line the street edge. Large floorplate stores are located behind or above smaller stores.*

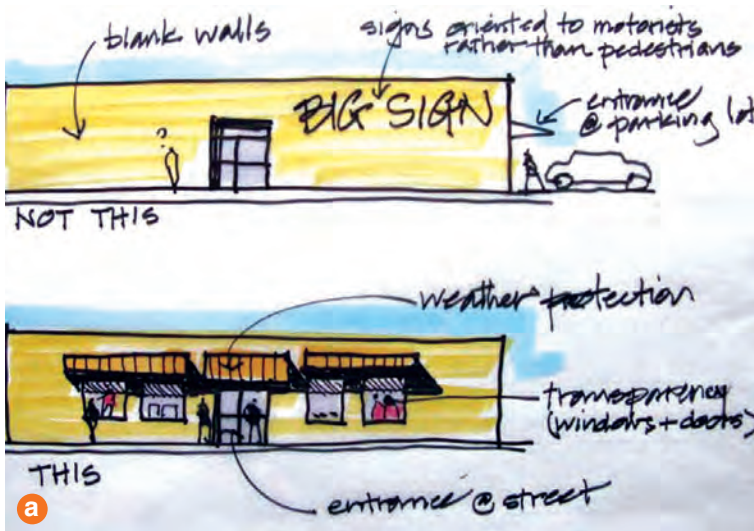
## Guidelines

- a) Large floor plate commercial developments should respond to the prevailing street character by incorporating frequent entrances and transparent shop front windows. Where new commercial is located in proximity to existing development, windows and entrances should be consistent with the established pattern of shop front entrances and windows along the street.
- b) For stand alone retail buildings over 2000 square metres in floor area, articulate the primary façade with a series of bays or shop windows to create a fine grained character to the frontage.
- c) In Downtown, large format commercial buildings should include a combination of these design strategies:
  - incorporate smaller shops wrapped around their edges
  - have their primary footprint located above the ground floor
  - include other uses above them, to better integrate these buildings and uses and make them more compatible with the mixed use character of Downtown.
- d) Maximum frontage along any street for single, large format stores is 30 metres.
- e) For sites over 4000 square metres in area, or for sites with multiple retail buildings:
  - Establish a street wall for a minimum of 40% of the site's frontage along public streets
  - Provide an interconnected internal walkway network that connects to all building entrances, parking areas, and adjacent public sidewalks:
    - Walkways should incorporate a pedestrian clearway (hard surface) of a minimum of 2 metres in width
    - 1.5-3.0 metres additional width to provide substantial low landscaping and/or canopy trees
    - Clearly demarcate crosswalks at all street and driveway crossings.
- f) Treat all facades facing primary, internal driveways as the primary building facade.

# LARGE RETAIL BUILDINGS



Large stores can be integrated with other uses to create more visually interesting and active frontages



Primary facades of large stores are articulated at a pedestrian scale with windows, weather protection and multiple entrances.



Surface parking for large format retail should incorporate substantial landscaping and internal walkways that directly link sidewalks on public streets to store entrances.

# COMMERCIAL - PUBLIC USE BUILDINGS

*These guidelines apply to private businesses that serve as public or quasi-institutional uses such as commercial art galleries, theatres, information services, and private utility buildings.*

## Objective

- » To ensure public use commercial buildings are articulated at a pedestrian scale, and contribute to the quality of the City Core on par with mixed use buildings.

## Guidelines

- Generally, public use commercial buildings should be built to the street edge and reinforce the street wall, consistent with other buildings on the block.
- Where set back from the street edge, public use commercial buildings should provide a public plaza or a landscaped forecourt as a public amenity.
- Commercial uses at ground level are required for frontages that would otherwise be blank, for example, back-of-house for a theatre.
- Public art treatments are required above ground level for frontages that would otherwise be blank, for example theatre fly towers or information technology buildings.
- Landmark architectural features are strongly encouraged at building entrances.



*Public use commercial buildings, such as this music school, should follow the general guidelines, and should reinforce City Core streets through their location, massing and articulation.*



*Public art treatment for upper level facades of a data centre.*

*These guidelines apply to industrial, manufacturing, storage and maker-space buildings.*

*Industrial buildings in the City Core should be focused on smaller scale uses oriented to design, craft, small batch production, and public sales/display. These use could include boat building, breweries, maker spaces, athletics (such as rock climbing, gyms), and artist space. Facilities requiring large warehousing, manufacturing, and frequent trucking are better located in the City's other employment areas.*

## Objective

- » To ensure industrial buildings are articulated at a pedestrian scale, and contribute to the quality of the City Core on par with mixed use buildings.

## Guidelines

- Maintain the urban character appropriate to the City Core including:
  - Buildings located to define street edges, public spaces and/or walkways
  - Frequent entrances
  - Minimum 40% transparency at ground level facing public streets and spaces
  - Significant building mass along their frontages to minimize gaps
  - A well connected pedestrian network of sidewalks and walkways
- Exhibit architectural features appropriate to industrial buildings situated within an active, pedestrian-focused urban area, which could include:
  - Simplified massing based on a rectangular building footprint
  - Flat, low slope or raised clerestory roof (sawtooth skylights)
  - Exposed or visible structural systems such as trusses and bracing
  - Vertically oriented painted metal or wood siding
  - Multi pane windows with metal frames
  - Use of substantial glass areas to the full height of the façade as accents
  - Oversized industrial roll up or sliding doors incorporating glass
  - Distinctive colour schemes and larger scaled signage.



*Vertical siding, exposed structure, distinctive sign and substantial full-height glass.*



*Sawtooth roof with clerestory windows.*



*Multi-pane windows*



*Visible structural elements*



*Frequent windows and entrances with industrial details.*

# TALL BUILDINGS

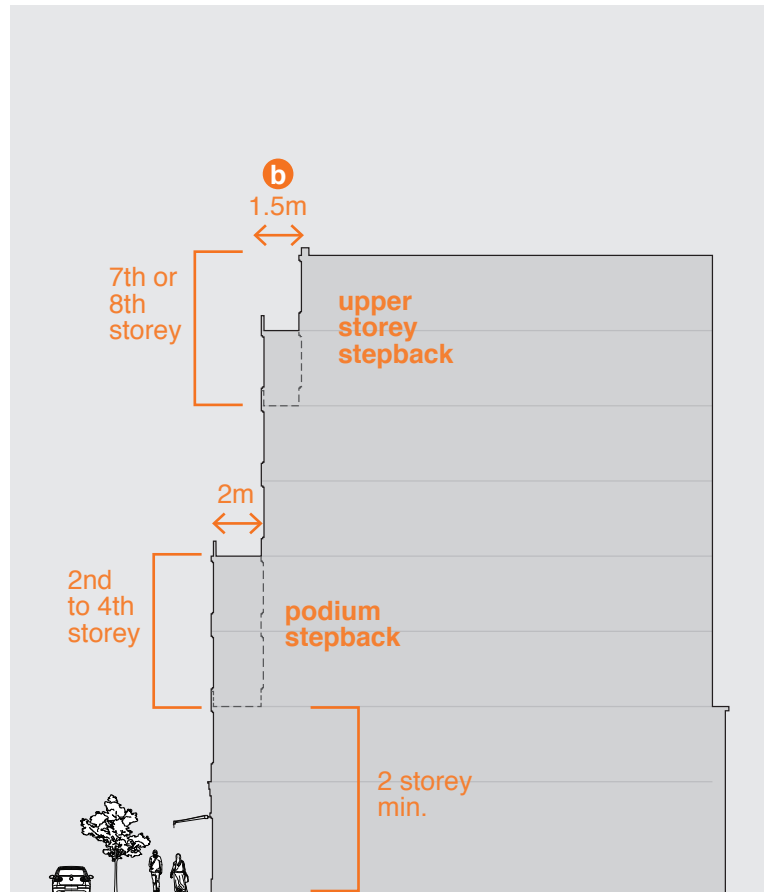
*These guidelines apply to buildings of 6 storeys or higher.*

## Objective

- » To ensure tall buildings, because of their high visibility and impact on the skyline, have a high quality of design.

## Guidelines

- The podium of tall buildings should relate to the streets and public spaces of the City Core, in particular following the Fine Grain guideline with respect to slab lengths. Above the podium, tall buildings have greater flexibility for massing and design.
- In addition to the 2m stepback above the podium, buildings of 7 or 8 storeys should ensure the top 1 or 2 storeys have a stepback of a minimum of 1.5 metres from the storey immediately below, facing publicly accessible streets and spaces.
- Where tall building sites are situated beside sites with lower height permissions, and/or where upper storeys of tall buildings will not be adjacent to other tall buildings, all sides of the upper storeys should be articulated with windows, balconies (if present) and architectural treatments consistent with publicly facing facades.
- Mechanical penthouses and elevator equipment should use materials and enclosure design that are consistent with the principal facade.



*Upper storey stepbacks apply to tall buildings.*



# DISTRICT GUIDELINES

- 1 MIDTOWN
- 2 DOWNTOWN
- 3 MARINA

The district guidelines provide additional direction for the specific form and character of development that is unique to each of the three districts. The district guidelines are in addition to the general guidelines, and are applied to sites based on their location within one or more of the districts.

# 1 MIDTOWN DISTRICT

## Objective

Midtown will undergo a transition to a primarily residential neighbourhood, with mixed use opportunities woven in. There will be a lot of ground contact housing, such as townhouses, in Midtown District. It will provide a wide range of housing choices and tenures, and great streetscapes will link Midtown with Downtown and the Marina District. Specific built form objectives of the Midtown District include:

- » To ensure residential uses address streets with a frontal appearance. Windows, doors, stoops, porches and front yard zones create a strong frame to the streetscape.
- » To limit gaps in the street wall and reinforce an urban character to the neighbourhood.
- » To ensure buildings are articulated with a traditional approach to window design and proportion.
- » To create a strong sense of place at the Five Corners gateway.

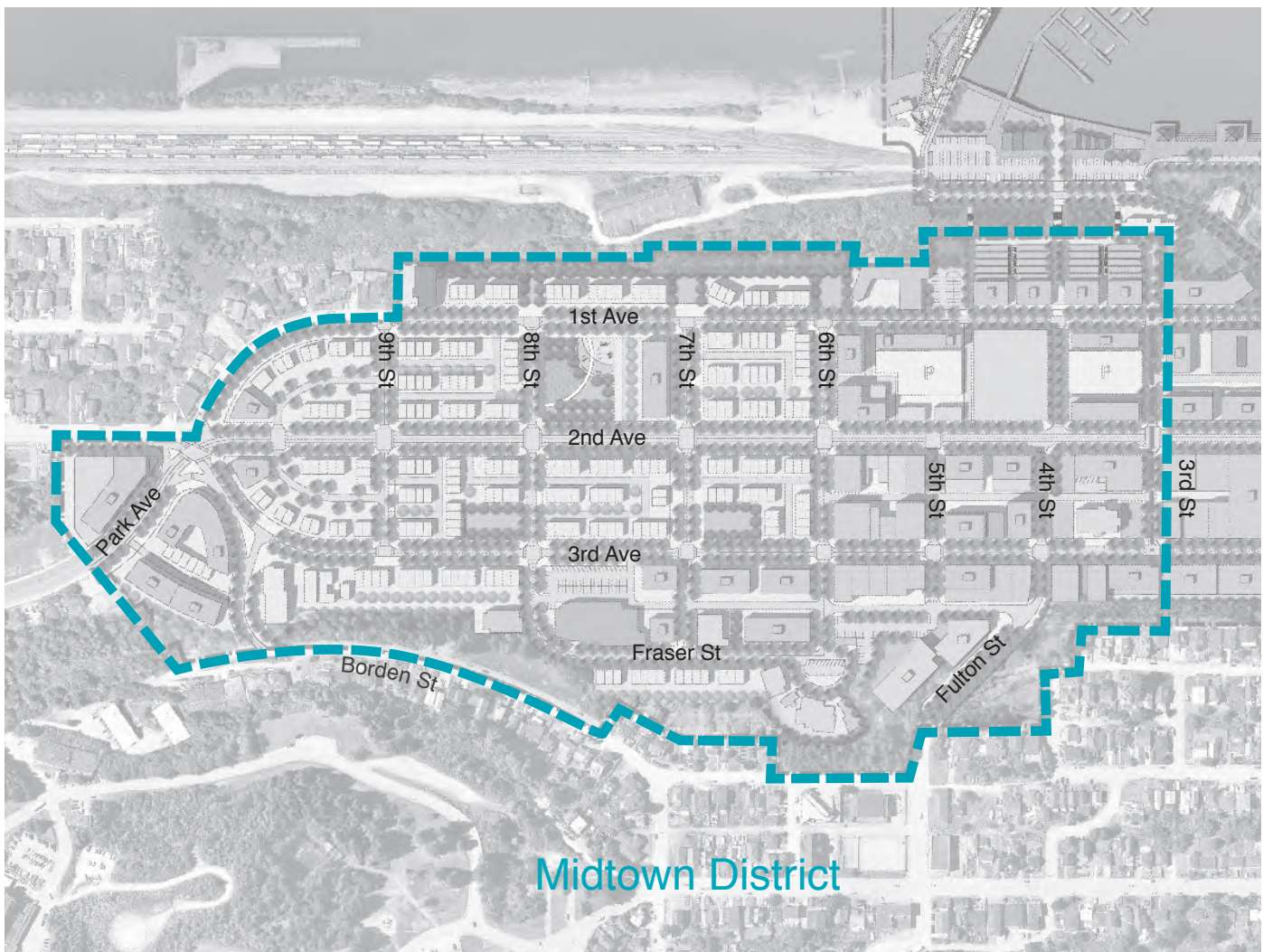
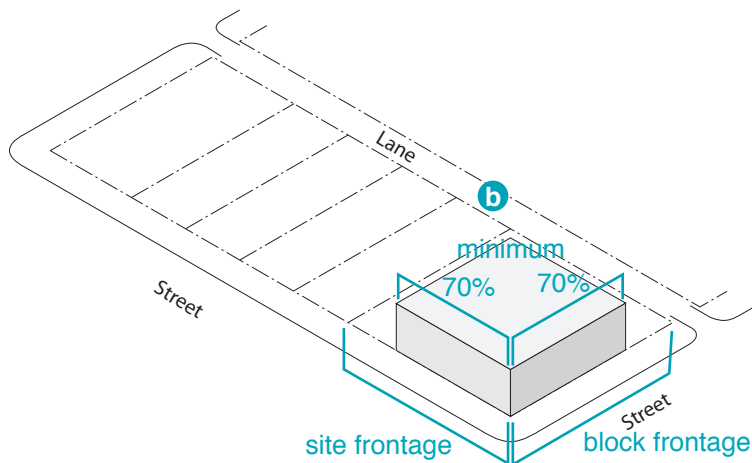


Figure 4: Midtown District Boundary, City Core Demonstration Plan From Prince Rupert 2030 The Vision



The street wall should define 70-100% of a site's frontage.

## Guidelines

- a) For streets that terminate at 1st Avenue, preserve the street-ends as open space to create and/or preserve views of the harbour, preferably for public use.
- b) Establish a street wall for a minimum of 70% of a site's frontage, or, along the length of a block.
- c) Where there are gaps in the street wall that are not used for pedestrian or vehicular access (for example, due to building code separation requirements), reinforce the street edge with landscaping such as low vegetation, visually permeable fencing, seat walls, bollards, or columns.
- d) All windows are to have a vertical proportion, although they may be subdivided by mullions into units with vertical, square or horizontal proportions.
- e) Upper storey windows should:
  - Be approximately the size and proportion of a traditional window
  - Include substantial trim, molding or sills
  - Be separated from adjacent windows by a vertical element
  - Be subdivided into smaller panes
  - Where they are grouped together to form large areas of glazing, separated with moldings or jambs.

## 2 DOWNTOWN DISTRICT

### Objective

Downtown will be the vibrant heart of the city. Great shopping on all streets will anchor mixed use buildings. A range of housing types and tenures will include small and large apartments. Improved linkages will be created between Upper Town and Lower Town, and important gateways to the city will be established. Specific built form objectives of the Downtown District include:

- » To ensure sidewalks and the pedestrian experience have the highest priority.
- » To ensure ground level uses are highly transparent and reinforce interesting, active streetscapes.
- » To minimize gaps in the street wall and reinforce an urban character to the district.
- » To ensure buildings are articulated with a traditional approach to window design and proportion.
- » To create a strong sense of arrival at two major city gateways: at McBride Street as it enters Downtown, at Rupert's Landing and all along the waterfront experience.

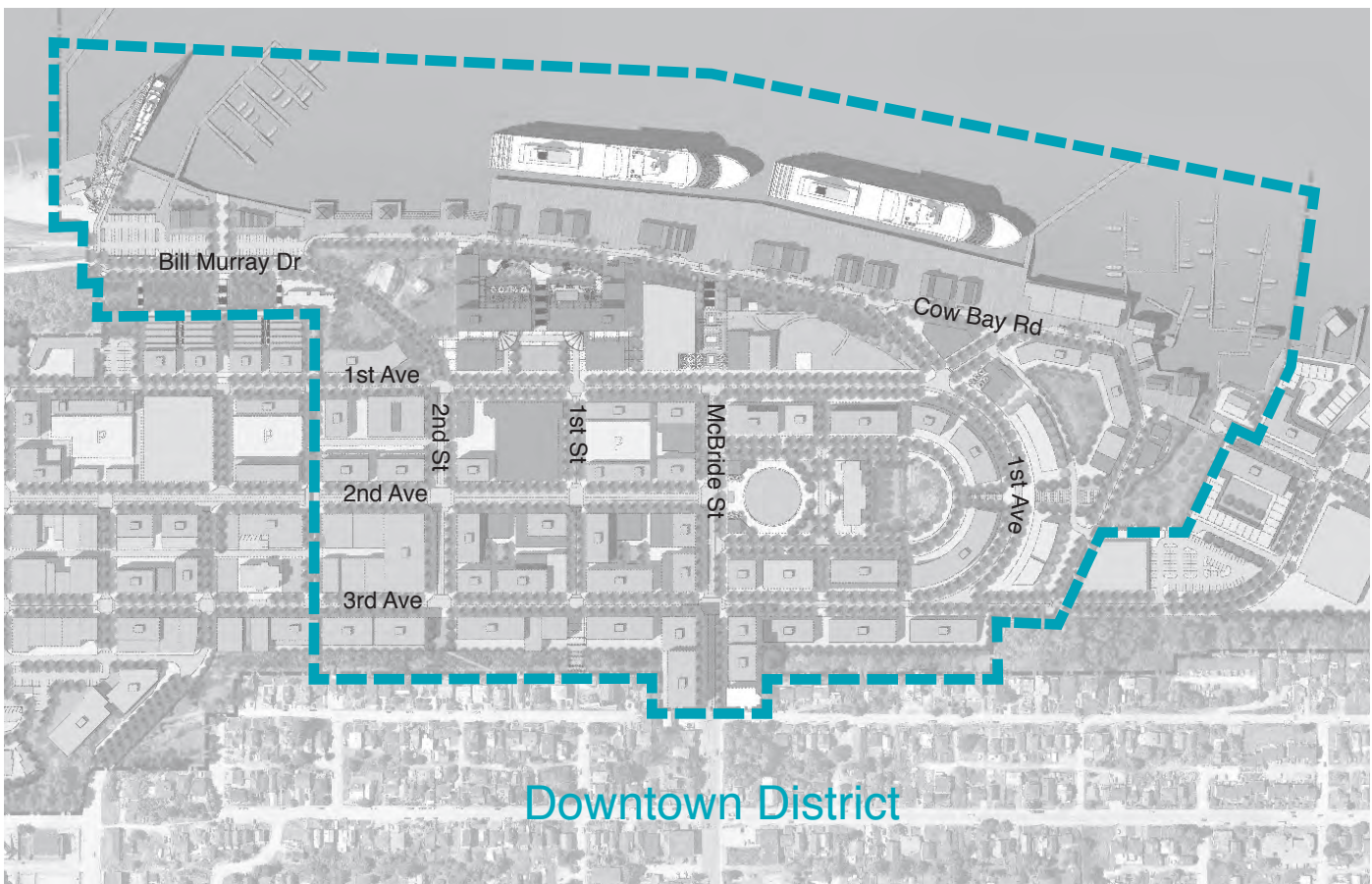
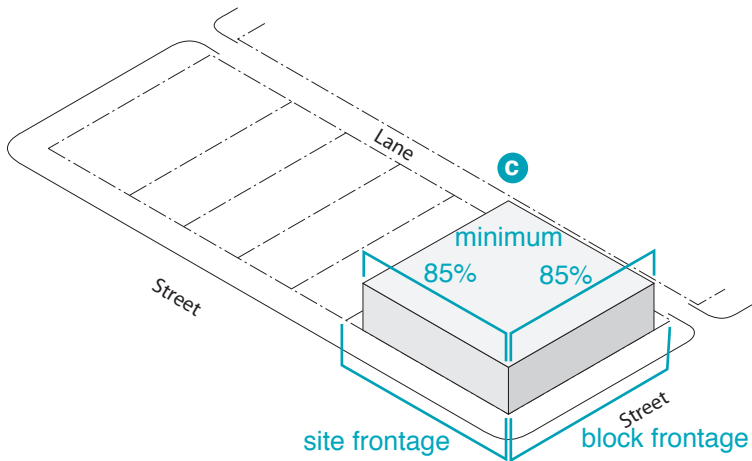


Figure 5: Downtown District Boundary, City Core Demonstration Plan From Prince Rupert 2030 The Vision



The street wall should define 85-100% of a site's frontage.



75% transparency at ground level facing public streets and spaces.

### Guidelines

- a) Build ground floor commercial uses to the front property line so that a continuous commercial street frontage and street definition is maintained.
- b) Vehicular entrances and curb cuts are not permitted along 3rd Avenue between 1st and 7th streets.
- c) Establish a street wall for a minimum of 85% of a site's frontage, or, along the length of a block.
- d) Create 75% transparency at ground level facing streets and public spaces.
- e) All windows are to have a vertical proportion, although they may be subdivided by mullions into units with vertical, square or horizontal proportions.
- f) Upper storey windows should:
  - Be approximately the size and proportion of a traditional window
  - Include substantial trim, molding or sills
  - Be separated from adjacent windows by a vertical element
  - Be subdivided into smaller panes
  - Where they are grouped together to form large areas of glazing, separated with moldings or jambs.

# 3 MARINA DISTRICT

## Objective

Through comprehensive redevelopment, the Marina District will transform into an eclectic mixed use area. The entire district will orient to the waterfront, with development creating routes to and along the water's edge. Comprehensive planning will ensure that there is ample green space throughout the district. The character of Cow Bay will continue through the shops, work spaces, and residences of the Marina District. There will be a range of housing options including luxury housing. Specific built form objectives of the Marina District include:

- » To ensure new development builds on the visual character of Cow Bay with contemporary design.
- » To create a fine grained network of publicly accessible streets and walkways that provide views of and access to a generous waterfront promenade along the ocean.
- » To provide a new waterfront destination park that has water-themed attractions for all.
- » To ensure streets and public spaces are planned to include trees and landscaping within the public realm and also on private property.
- » To limit gaps in the street wall and reinforce an urban character to the district.



Figure 6: Downtown District Boundary, City Core Demonstration Plan From Prince Rupert 2030 The Vision

## Guidelines

### Public Realm

- a) New buildings should be organized by a publicly accessible network of new streets and pedestrian spaces, designed to look and feel publicly accessible even if they are privately owned. There shall be no restrictions on public access to these spaces if privately owned.
- b) There may be additional, subordinate laneways, walkways and internal courtyards that are private.
- c) New public streets and pedestrian spaces should be aligned to provide views of and access to the waterfront from George Hills Way, including from development on the opposite side of George Hills Way.

### Streets

- d) Public streets and pedestrian spaces should generally be 60 to 80 metres apart to create a fine grain block network.
- e) New streets should include a travel lane in each direction, an on-street parking/layby lane on at least one side of the street, and sidewalks on each side of the street that are a minimum of 4.5m in width including the planting/furnishing zones. Additional on-street parking lanes, turn lanes and sidewalk space may be provided.
- f) Each sidewalk should include:
  - A continuous row of street trees that, at maturity, will form a continuously connected canopy.
  - Low vegetation within open planters where tree soil volumes are located.
  - Tree soil volumes of a minimum of 30 cubic metres per tree, within 1.2 metres of the surface. Soil volumes may be achieved under hard surface through soil cells or other methods, or by utilizing adjacent front yard landscape areas where roots have unimpeded access to those areas.
  - Benches every 50 metres.
  - Pedestrian scaled street lighting.

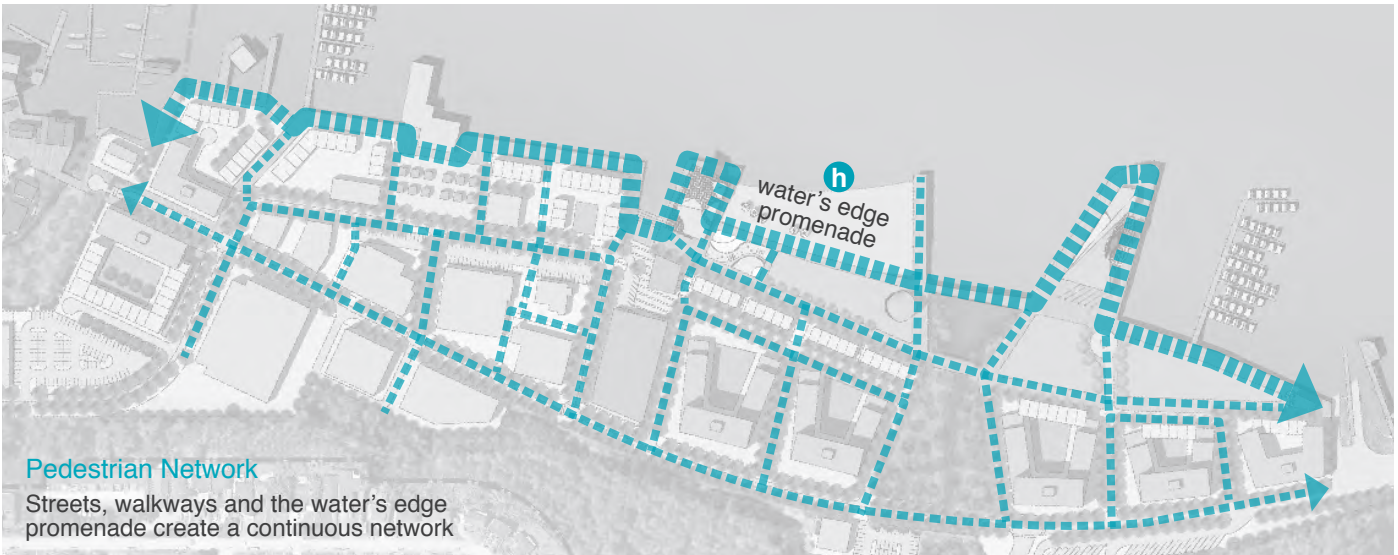
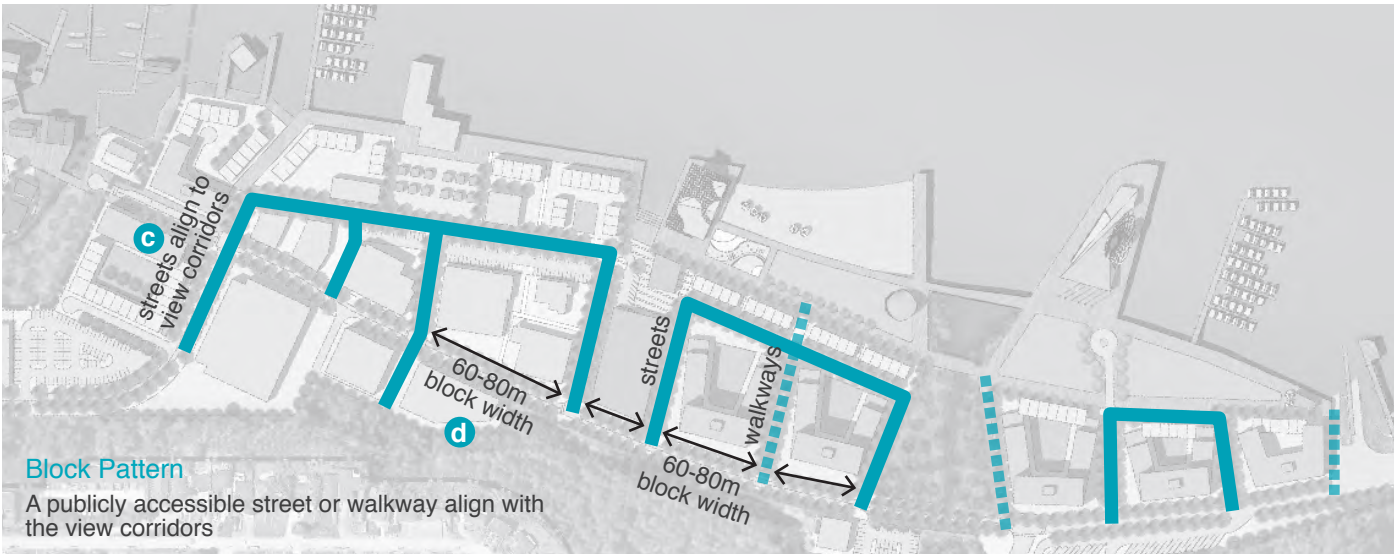
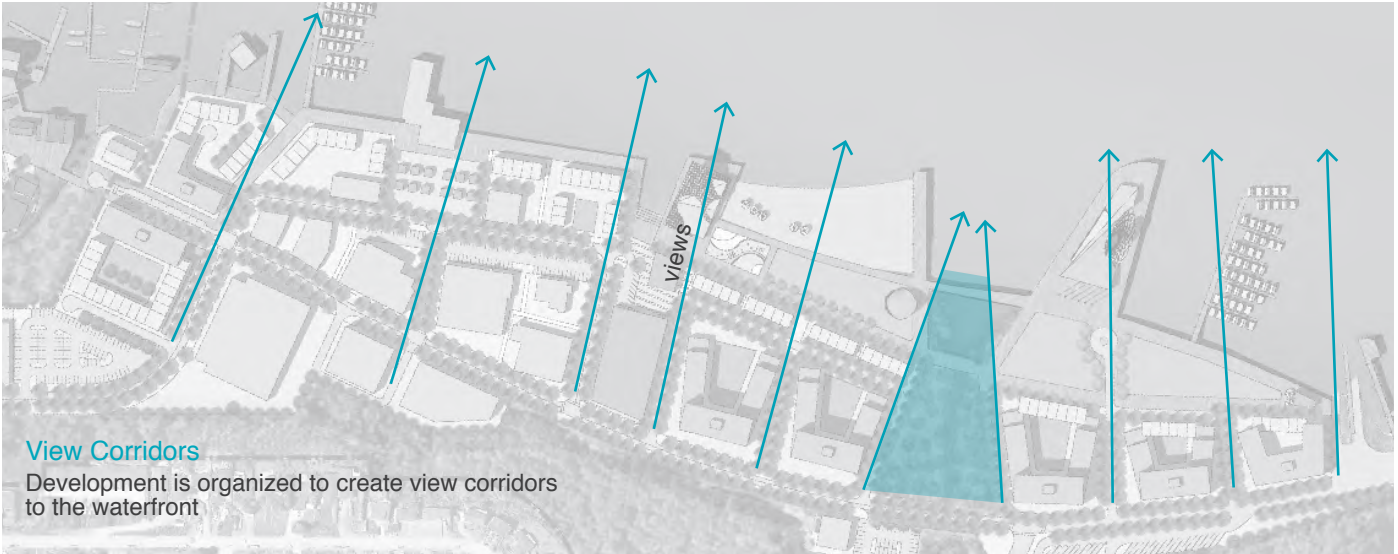
### Pedestrian Spaces

- g) New pedestrian spaces may include mid-block pedestrian connections, mews, courtyards, promenades, shared spaces and new waterfront parks. They should be designed to be publicly accessible and include walkway connections to all surrounding sidewalks and building entrances.
- h) Continuous public access to the water's edge should be provided including a public promenade.
- i) Linear pedestrian spaces such as mid-block connections should be a minimum of 9 metres in width and should include:
  - At minimum, a continuous row of street trees that, at maturity, will form a continuously connected canopy.
  - Low vegetation within open planters where tree soil volumes are located.
  - Tree soil volumes of a minimum of 30 cubic metres per tree, within 1.2 metres of the surface. Soil volumes may be achieved under hard surface through soil cells or other methods, or by utilizing adjacent front yard landscape areas where roots have unimpeded access to those areas.
  - Benches every 50 metres.
  - Pedestrian scaled lighting.



*Pedestrian connection between development blocks.*

# 3 MARINA DISTRICT



i



Water's edge promenade.



Hard and soft surfaces, walkways, covered and flexible performance space.



Public art and cultural heritage interpretation.



Activities for all ages.

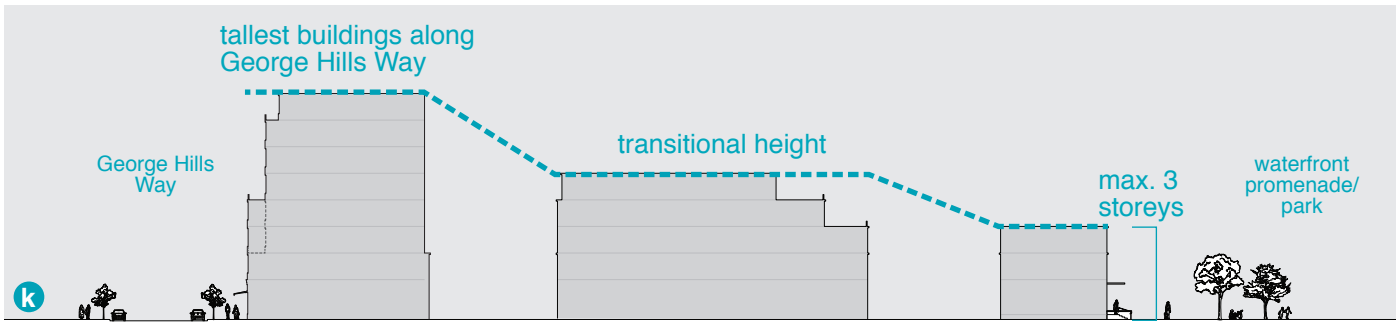


Water themed recreation.

## Park

- j) A destination waterfront park should be designed with the following elements:
- Continuous water's edge promenade or public access to the water's edge.
  - A network of walkways connecting all adjacent building entrances, sidewalks and pedestrian spaces with the water's edge promenade.
  - Incorporation of Hays Creek valley and outlet to the ocean.
  - Large, flexible, multi-use areas in both hard and soft surfaces that can be used for events and general recreation.
  - Provision of covered performance space with access to power, such as a bandshell or gazebo.
  - Structural elements for weather protection such as canopies and wind breaks.
  - Activities for all ages including play elements for children and youth.
  - Water-themed recreational elements such as an artificial beach or an area for people to get to water level.
  - Public art.
  - Cultural heritage interpretation of former uses and stories along the waterfront.
  - Seating (30 metre maximum spacing), pedestrian lighting, waste receptacles, wayfinding.
  - May include compatible commercial uses such as cafes or food vendors.
  - May include compatible public uses such as cultural centres, galleries or museums.

# 3 MARINA DISTRICT



Intermediate building height between buildings along the waterfront and taller buildings along George Hills Way.

## Buildings

k) Establish a transition in building height to the waterfront:

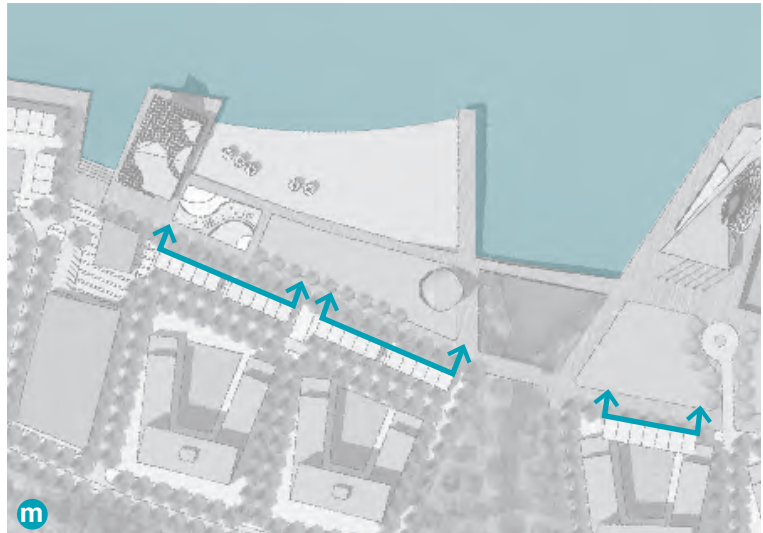
- The buildings directly adjacent to and facing the public space along the water's edge should be a maximum of 3 storeys in height.
- The tallest buildings should be located along George Hills Way.
- There should be at least one intermediate building height between buildings at the waterfront and buildings of 6 or more storeys in height.

l) Building terraces are encouraged to create a transition to the waterfront and public spaces, and to provide generous amenity space for residents.

m) Residential uses at ground level adjacent to the waterfront promenade or a waterfront park should be oriented to front onto a street or walkway. Ground floor units should have individual entrances from the street or walkway. Buildings should not flank or have reverse frontage onto the waterfront.

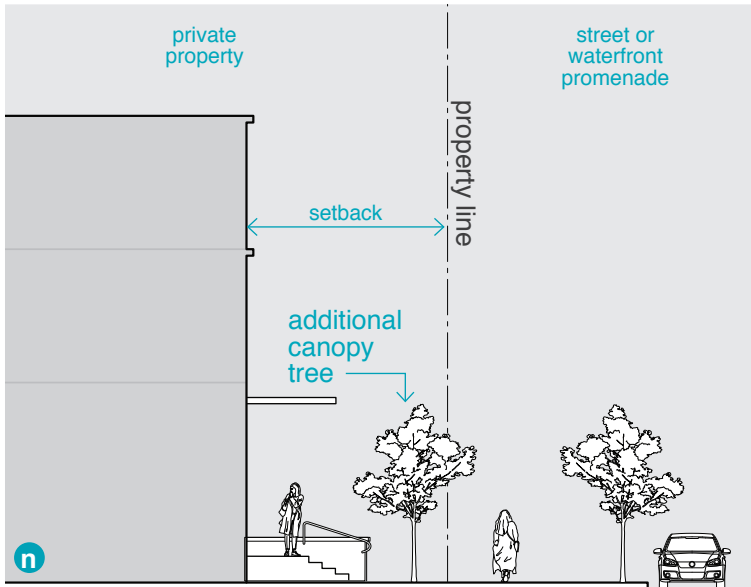


A terraced building design steps down to the waterfront and creates amenity spaces for building residents.

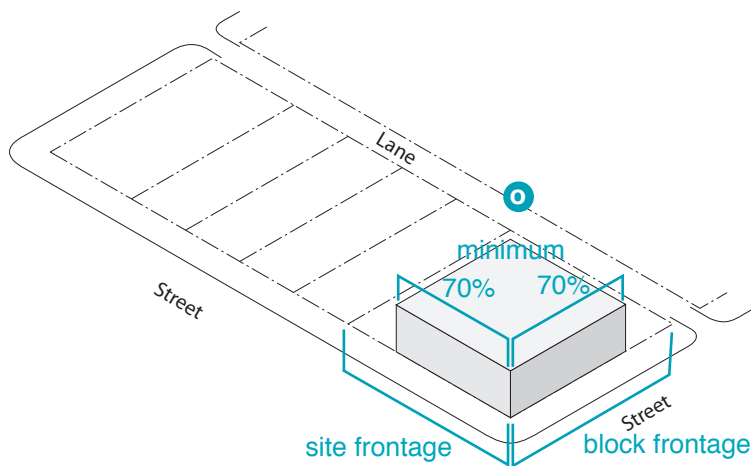


Buildings face the waterfront.

### 3 MARINA DISTRICT



A tree on private property, within the front yard, will help create green streets in the Marina District.



The street wall should define 70-100% of a site's frontage.



60% transparency at ground level facing public streets and spaces.

- n) All residential uses at ground level should be set back a minimum of 3.5 metres and a maximum of 6.0 metres from public streets and the waterfront promenade. Front yard landscaping within this setback should include canopy trees compatible with the street trees in the sidewalk zone.
- o) Establish a street wall for a minimum of 70% of a site's frontage, or, along the length of a block, for all frontages facing publicly accessible streets and spaces.
- p) Create 60% transparency at ground level facing streets and public spaces.

### Character

- q) To be consistent with the visual character of Cow Bay, development shall include a well designed combination of the following elements:
- Simple massing based on a rectangular building footprint
  - Sloped roof for buildings of 3 storeys or less, with architectural elements such as dormers and towers that project into the roof plane
  - Simple colour scheme for individual buildings, with many different colours in the collective
  - Significant ground level fenestration
  - Large windows
  - Sloped-roof canopies over entrances and sidewalks with an industrial character
  - Projecting blade signs, upper level signs, and large signs or murals on exposed facades
  - Surface mounted lighting.
- r) Ground level uses should be as diverse as possible and mixed over short distances.
- s) Each use should have a separate building entrance that is articulated through massing and design to express its unique interior use or tenant.
- t) Diverse uses should share pedestrian access from adjacent streets and walkways, as well as share loading and servicing zones.



# CHARACTER AREA GUIDELINES

- 1 HERITAGE CORE
- 2 CIVIC NODE
- 3 COW BAY
- 4 WATERFRONT

The character area guidelines provide additional direction for the specific form and character of development that is unique to each of the four character areas. The character area guidelines are in addition to the general guidelines and district guidelines, and are applied to sites based on their location within one or more of the character areas.

# 1 HERITAGE CORE

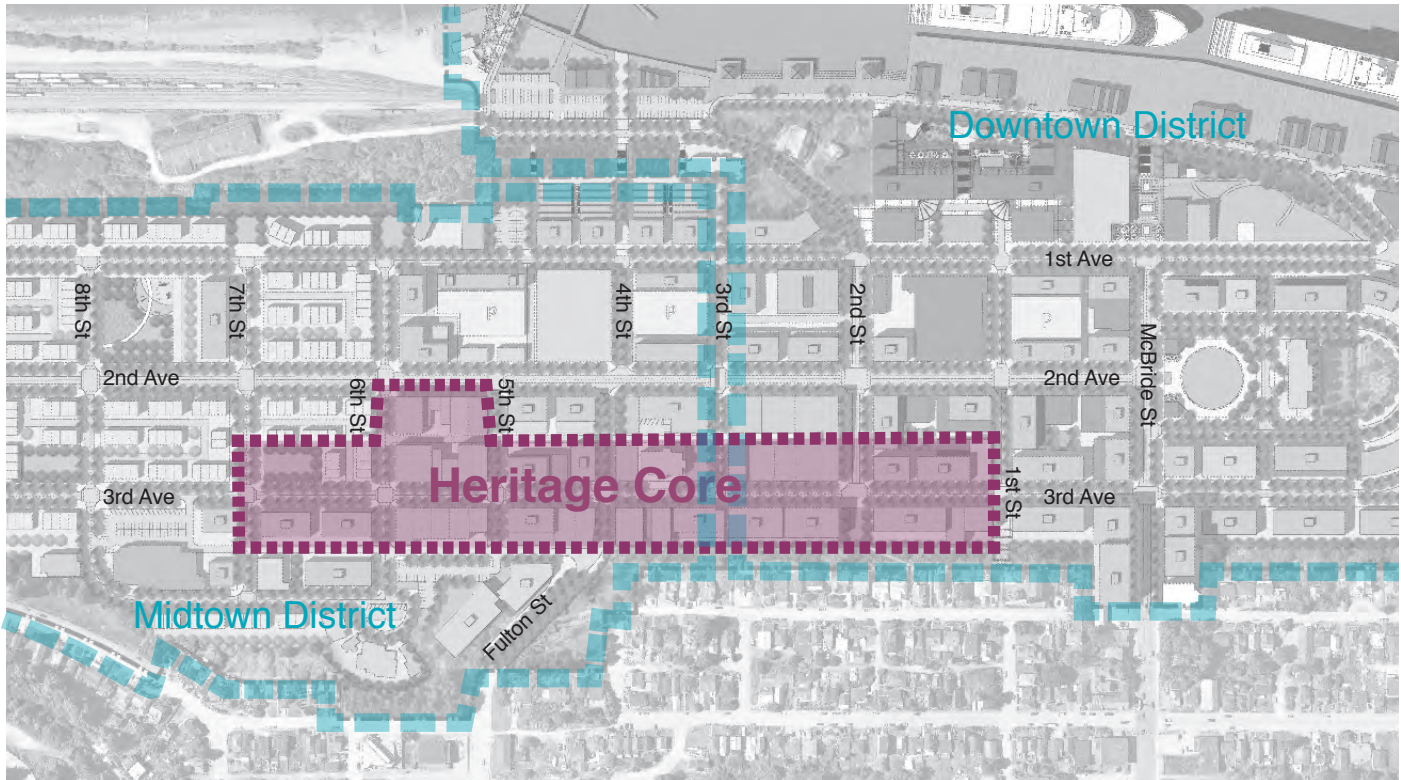


Figure 7: Heritage Core Character Area Boundary

## Introduction

The heritage core is centred on 3rd Avenue, Downtown Prince Rupert's traditional retail high street. Local shops at street level have narrow frontages, frequent entrances, and enticing shop windows. This area also includes an eclectic mix of notable heritage buildings ranging in style from the simple and plain to the exuberant. The building and streetscape character in this area has and will continue to play a paramount role in creating a unique identity and sense of place for downtown Prince Rupert and the City as a whole. It is important that renovations and new developments maintain and enhance the established use and defining characteristics of this area using the principles of design from this area's architectural past. The street relationship and organization of architectural elements of former buildings, and of the remaining heritage buildings, are good precedents to emulate in the design of infill development.

The principles of design exhibited by historic buildings can be separated from the style of their architectural expression and detailing. The following guidelines do not promote historic styles over modern expression. Both are equally capable of responding to the principles of building design desired for the area. No single architectural style is prescribed for new construction, either for new infill buildings or for significant additions to heritage buildings (e.g. additional storeys above an existing heritage building).

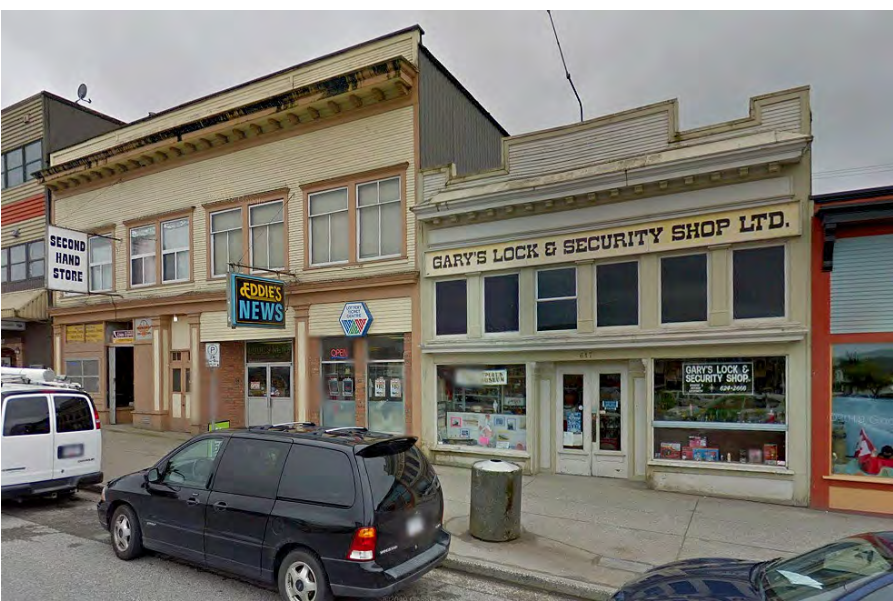
However, for restoration of heritage buildings, archival photographs of historic downtown Prince Rupert should be used as a design resource to assure authenticity in the replication of missing, or covered over, detail on historic structures, and to guide in the design of appropriate styling details. The extensive heritage inventory (Kalman, 1983) is an excellent resource for determining heritage features of buildings whose facades have been covered over.

# 1 HERITAGE CORE



## Objective

- » To reinforce the traditional 2 to 3 storey street wall of buildings along 3rd Avenue that contributes to the intimate, pedestrian scale of the street.
- » To reinforce the continuity of building facades and character of street fronting retail development throughout.
- » To restore and replicate significant architectural detail where such detail contributes to and reinforces the area's character.



# 1 HERITAGE CORE

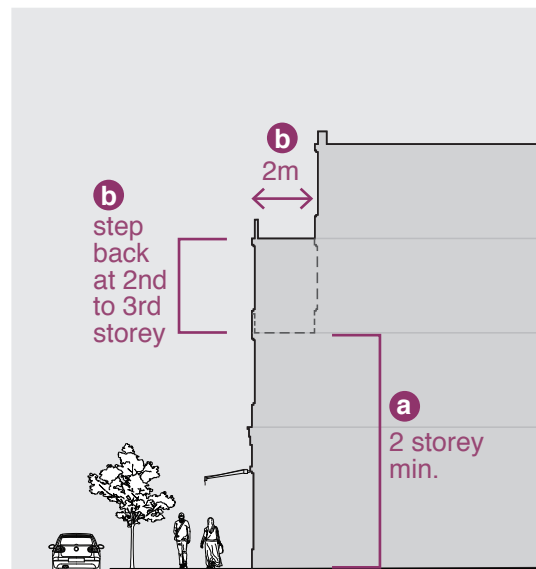
## Guidelines

### Street Wall

- a) Create a minimum 2 and maximum 3 storey street wall at the property line that occupies the full frontage of development.
- b) An upper storey step back of a minimum of 2m should apply to portions of the building taller than the street wall, including for new additions above existing heritage building facades.
- c) Establish a rhythm of buildings along the street through the use of vertical elements and a fenestration pattern arranged into modules that reflect the 7.6 m or 15.2 m façade rhythm of the historic streetscape.
- d) Provide a continuous retail frontage, with shop entrances every 7.6m (preferred) or 15.2m (maximum).

### For Existing Heritage Buildings

- e) Original, historic, building materials should be retained whenever possible during restorative renovations. Historic material should never be covered with modern materials.
- f) Uncover and refurbish historic materials that have been covered over due to a previous renovation, to as near original condition as possible.
- g) Signs on heritage buildings should be compatible in terms of character, colour and material and should never obscure heritage details.



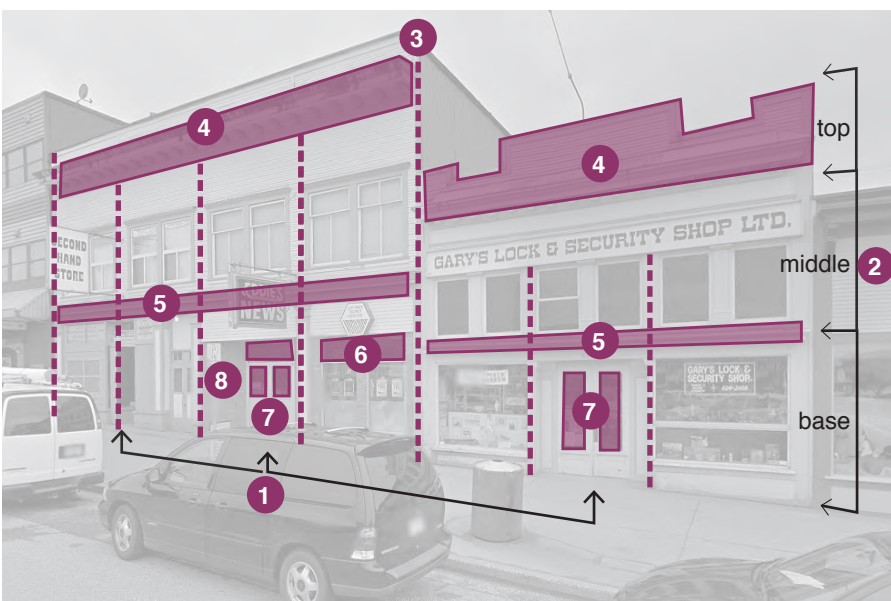
2m step back above the 2nd or 3rd storey for all buildings.

# 1 HERITAGE CORE

## Character

- h) New buildings should be designed to be consistent with the design principles of Prince Rupert's historic buildings in the downtown, by providing a well designed combination of the following character defining elements.

## Character Defining Elements



- 1 Store entrances 7.6m - 15.2m apart
- 2 Visually strong base, middle and top
- 3 Rhythm of vertical elements in the facade, particularly above ground level, through techniques such as pilasters, columns, projecting bays and windows, recessed balconies, window proportion and datum lines
- 4 Cornice or datum line at the street wall height; may include up to 1.2m parapet or false front
- 5 Cornice or datum line above the ground floor, consistent with but subordinate to, the cornice or datum line for the street wall
- 6 Transom windows at ground level
- 7 Glazing in retail entry doors
- 8 Minimum 1.2m recessed entry space for retail entrances, trapezoidal plan (preferred) or rectangular (acceptable)
- 9 Weather protection strongly encouraged
- 10 Architectural features such as lintels, sills, decorative bands, mouldings, and trim, that are consistent with the principal building expression, are encouraged
- 11 Vertically oriented upper storey windows organized into a repetitive pattern along the facade
- 12 Punched (recessed) upper storey windows
- 13 Materials and colours that are compatible with a heritage context, see next page.

# 1 HERITAGE CORE

## Materials for the Heritage Core

### Preferred

- Natural stone laid in regular, coursed, patterns
- Brick in traditional sizes, colours and patterns, including decorative techniques such as corbelling and inset panels
- Painted wood board siding in traditional sizes, patterns and methods of application, including corner boards, trim and mouldings
- Sawn wood shingles, including in decorative patterns such as fish scale and diamond
- Metal, including non-ferrous metals such as copper, brass, bronze, zinc
- Pre-cast concrete, if used for traditional decorative elements such as flashings, cornices, brackets, finials, capitals and datum lines
- Clear glass

### Acceptable

- Stucco with a smooth, sand float finish in muted colours (preferably earth tones) mixed directly into stucco mortar. The amount of stucco on front (primary) façade should not exceed 25% of the total surface area of opaque elements
- Precast concrete panels

### Strongly discouraged

- Significant use of stucco, and stucco with decorative finishes or bright colours
- Random coursed stone, brick or cobbles
- Modern oversized or undersized brick forms
- Artificial stone or brick veneer
- Galvanized metal siding
- Synthetic materials such as vinyl, fiberglass, plastics and exterior insulation and finish system (EIFS) panels
- Asphalt shingles
- Unfinished or rustic wood, or wood sheet goods
- Pastel colours

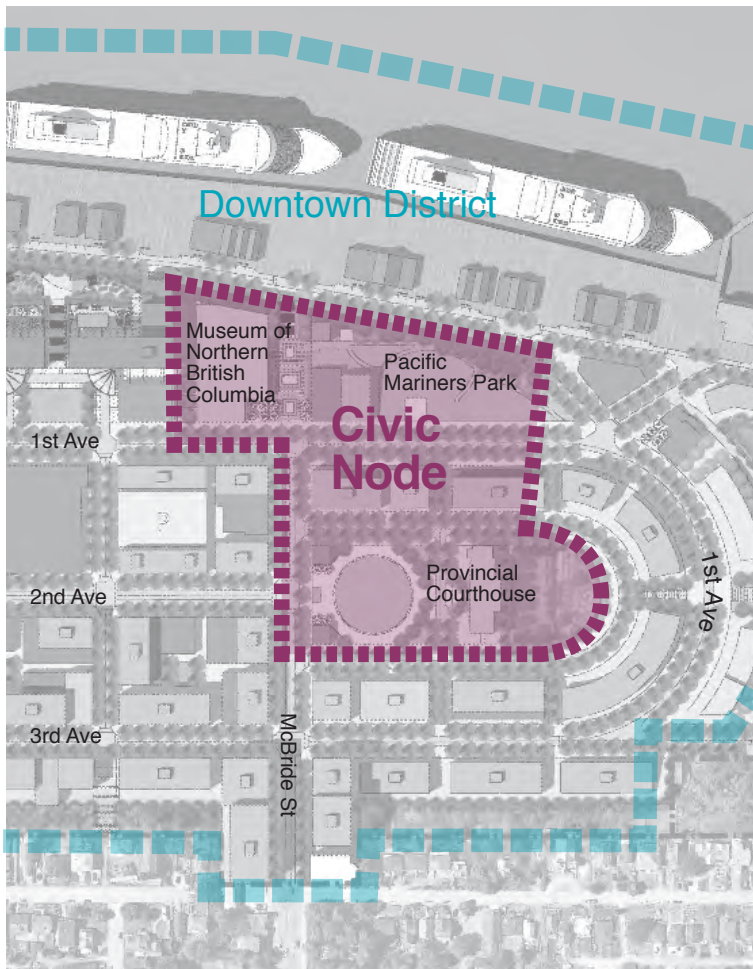


Figure 8: Civic Node Character Area

### Introduction

The Civic Node includes a number of notable and architecturally distinct buildings. These include two of the most architecturally prominent buildings in the downtown: The Provincial Court House building and the Museum of Northern British Columbia. It also includes the most prominent and important parks and open spaces in the downtown, including the courthouse lawn, the Sunken Garden, Pacific Mariners Park and the McBride Street plaza. There are several pieces of significant public art within the public spaces central to the City's history and identity. The Provincial Court House Building and lawn are an important focal point of the Brett and Hall Plan and a view terminus to 2nd Avenue.

The Museum of Northern British Columbia is located in a cedar traditional longhouse-style building fronting on to 1st Avenue and overlooking the ocean. This iconic building incorporates massive exposed cedar timbers and contemporary glass art. Associated with the Museum of Northern B.C. is the Museum Carving Shed, a long house style timber building clad with cedar planks located along Market Place adjacent to the Provincial Court House

Buildings in the cultural and civic sub area have greater flexibility in their siting, orientation and design due to their special status as distinct buildings with cultural significance. Buildings in this area are more suitable as stand alone buildings that do not touch each other but rather, stand out as individual buildings oriented towards prominent natural or landscape features. As such, development in this area that has a significant public benefit is exempt from the general guidelines contained in this document, provided it ensures a pedestrian orientated and a high quality design treatment of the public realm.

### Guidelines

- a) Buildings and landscaping located along 2nd Avenue and adjacent to the courthouse lawn should preserve views to these features.
- b) Promote pedestrian connectivity between public spaces and civic buildings through enhanced pedestrian walkways.

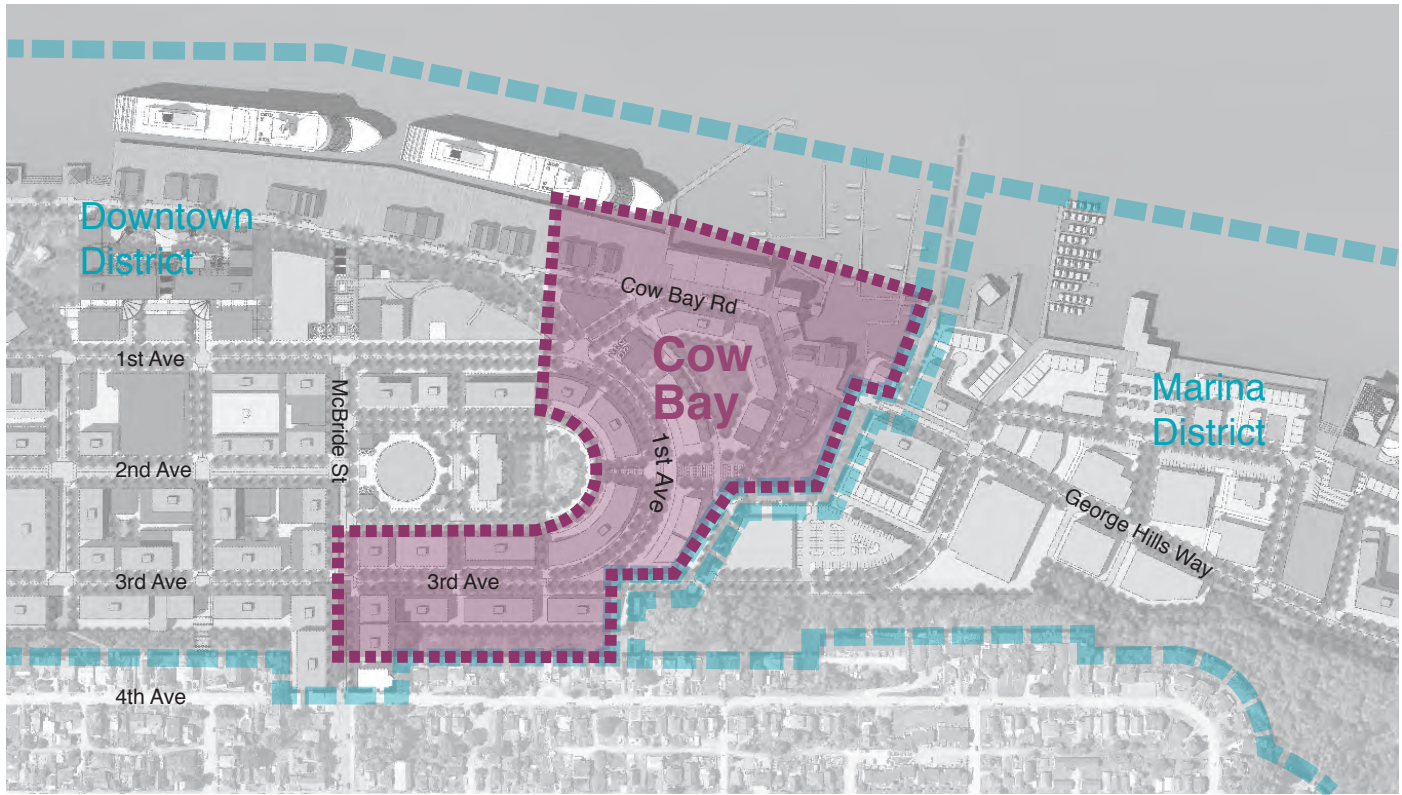


Figure 9: Cow Bay Character Area

### Introduction

Cow Bay has evolved into a distinct part of downtown Prince Rupert with a unique character and special sense of identity. It has a diverse mix of industrial, commercial, marine, tourism, recreational and residential uses. Cow Bay successfully balances the elements of a working waterfront with a retail, restaurant, recreational and tourism focus. Because of this mix of uses and the authenticity of the harbour environment, Cow Bay is interesting and attractive to people and has become one of the defining symbols of the city.

The guidelines that have been in place for Cow Bay since 1986 have shaped the creation or renovation of many distinct buildings that contribute to this area's charm. The following guidelines maintain the simplicity and ease of use of the former guidelines while being consistent with the broader objectives of the City Core.



## Objective

- » To reinforce the established unique urban waterfront character of Cow Bay, containing a mix of marine related light industrial and commercial activity, specialty entertainment and shopping, with opportunities for public recreation and access to the waterfront.
- » To maintain the physical features, businesses and activities that contribute to Cow Bay's unique character.
- » To ensure new development and activities are compatible with existing development.
- » To reinforce Cow Bay's role as a specialty commercial area that is complementary to, not competitive with, downtown.
- » To create attractive, functional links between Cow Bay, downtown and Prince Rupert's waterfront.
- » To ensure the elements of the working port, including light industrial, marine and commercial activity are visually accessible to the public and contribute to the area's character, while maintaining safety and operational requirements for those uses



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### Guidelines

#### Site Design

- a) Site buildings to provide views and public access to the water's edge.
- b) Buildings at the water's edge should be linked to each other and to adjacent walkways and parking areas by boardwalks or elevated docks.
- c) Painted wood or steel handrails should be tied in with boardwalks and decks, with low level illumination for safety. Avoid 'residential' character balustrades and aluminum.
- d) Retain elements of the area's industrial past such as old rail tracks and marine artifacts in the design of landscaping.

#### Buildings

- e) Existing structures in the Cow Bay Core Area should be retained wherever possible.
- f) Renovations of existing structures should respect and enhance the original building character.
- g) New buildings, where feasible, should be built on piling and deck rather than on landfill.
- h) Large development should generally appear to consist of many small buildings rather than large singular buildings.



## Character

- i) New buildings should be designed to be consistent with the existing character of Cow Bay, by providing a well designed combination of the following character defining elements.

## Character Defining Elements



- 1 Simple massing based on a rectangular building footprint
- 2 Sloped roof
- 3 Architectural elements such as dormers and towers project into the roof plane
- 4 Simple 2 or 3 colour scheme, with one colour for siding and another for trim; no earth tones or greys
- 5 Horizontal siding in shiplap or shingles; or metal cladding
- 6 Door colour same as trim colour
- 7 Corner board trim
- 8 Significant ground level fenestration
- 9 Windows subdivided into smaller units by mullions or transoms
- 10 Bay windows
- 11 Windows framed by trim
- 12 Sloped-roof canopies over entrances and sidewalks; industrial character
- 13 Projecting blade signs
- 14 Upper level signs
- 15 Surface mounted lighting

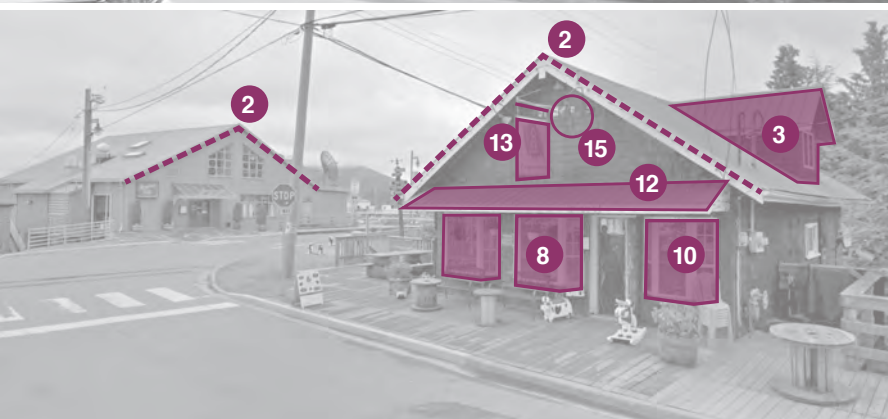




Figure 10: Waterfront Character Areas

## Introduction

The waterfront in the City Core extends from the edge of the Midtown District, and continuously along the Downtown and Marina Districts. It includes the public realm as well as the buildings that address the public realm along the water's edge. The waterfront includes marine, industrial, commercial and residential uses mixed in proximity. This dynamic interplay of uses, combined with the water's natural attraction for people, makes the waterfront a prime destination for both day to day and event based experiences. The waterfront is also a major arrivals point and key experience for visitors. A great waterfront makes a significant contribution to the quality of life of the city.

The most important principle for Prince Rupert's waterfront is to establish a continuous pedestrian route that includes plenty of places to sit and occasional weather protection at or near the water's edge. This will take many years to establish. There are areas where private land and industrial uses prohibit public access to the water. These should be recognized as vital economic generators for the city, and often, as interesting and authentic attractions of the working waterfront for tourists and visitors. In the interim, a continuous pedestrian route should be established around these interruptions, using sidewalks or temporary walkways.



### Objective

- » To provide continuous public access to the water.
- » To preserve the authenticity of the working waterfront in its uses and robust, functional structures.
- » To ensure new buildings have a pedestrian scale and address the waterfront in their architectural composition.



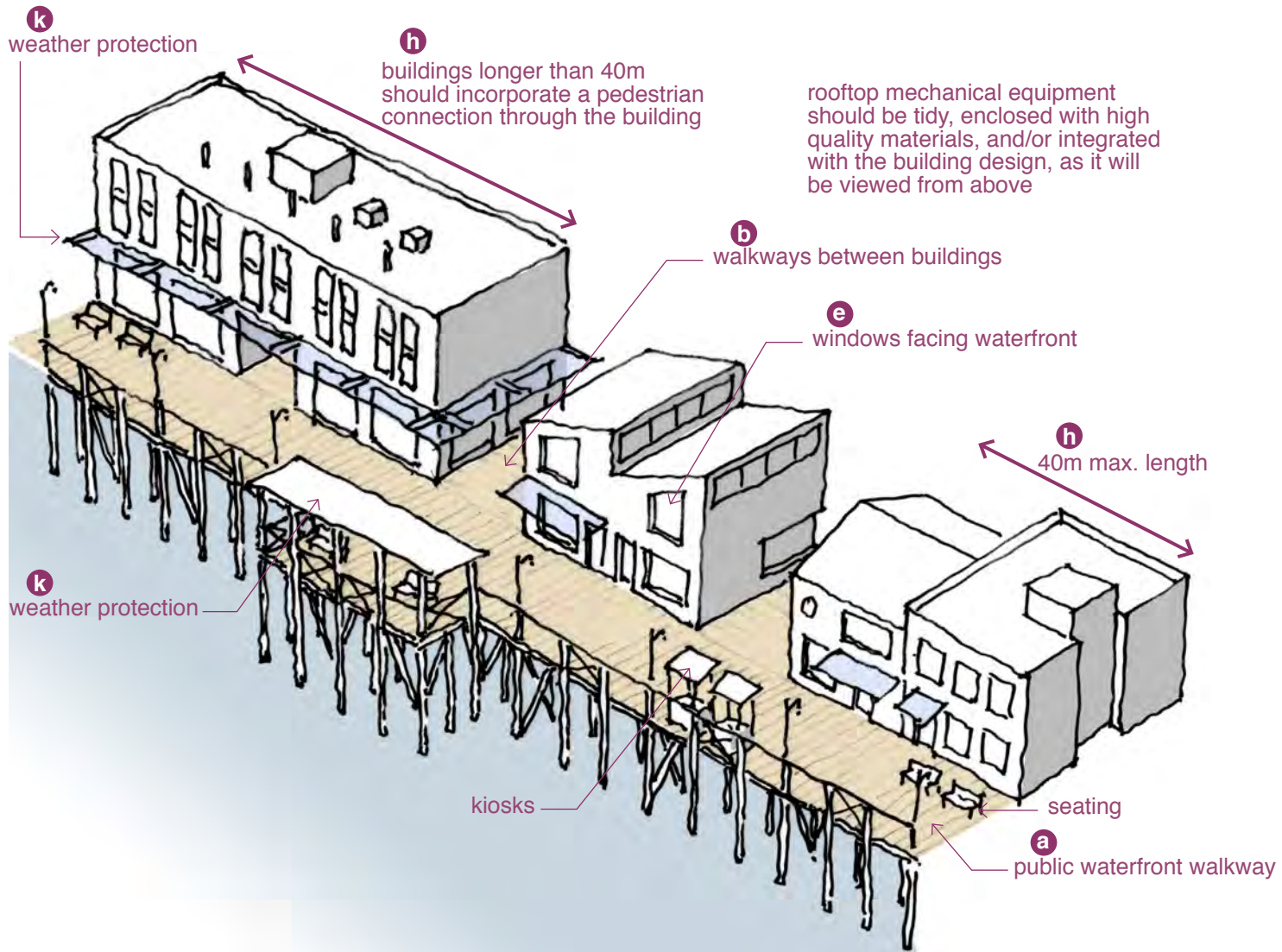
### Guidelines

- a) A public promenade punctuated with public attractions, seating areas, kiosks and weather protection should be provided at the water's edge. The only development permitted between the promenade and the water's edge should be existing buildings, and new buildings that are publicly accessible such as markets, galleries, museums and cultural centres.
- b) New development that is built on marine piles may be located at the water's edge and should provide the publicly accessible waterfront promenade with a decking system. The deck should link with the promenade on adjacent properties, and all surrounding sidewalks and building entrances.
- c) Access points to marinas, docks, floating villages, boat launches and other potentially private uses on the water should not obstruct the public promenade or views of the waterfront.
- d) Multiple walkway connections should be provided between the waterfront promenade and adjacent streets, sidewalks and development sites, to promote accessibility and connectivity.
- h) Multiple buildings are preferred over large buildings. Limit building lengths to less than 40 metres in length parallel to the water's edge. For longer buildings, provide a publicly accessible connection through the building at ground level to the waterfront promenade, for example with a breezeway or arcade.
- i) Step backs are encouraged to be incorporated in terraces that step down towards the waterfront.
- j) The front facades of residential buildings facing a pedestrian connection or mews should be a minimum of 15 metres apart.
- k) Weather protection is strongly encouraged along facades facing the water.
- l) Materials and construction methods appropriate for buildings and public realm elements include:
  - Large, heavy and durable natural materials in rustic finishes, such as large stones, timbers, and planks
  - Weathering steel or steel with marine grade paint
  - Exposed joinery and fasteners
  - Protective cages
  - Cross bracing or diagonal structural members

### Buildings

- e) Buildings facing the waterfront should incorporate substantial transparency in their façade design.
- f) Building styles should generally be contemporary in design (with the exception of Cow Bay) and should not mimic historical architectural styles. They may reference historical forms and details where those are interpreted for their modern context.
- g) Significant cultural buildings such as a Tsimshian centre may adopt either modern or historically accurate styles of architecture.
- m) Structural elements should be simple and express their function, for example marine piles supporting buildings, docks and boardwalks.
- n) Safety elements such as railings should be consistent in design language across the waterfront. They should be part of or fastened to their primary structures and have an open character (80% voids) to maintain views of the water.

# 4 WATERFRONT







# GLOSSARY

The glossary explains terms used in these guidelines.

## Articulation

The design elements of a building's façade inclusive of materials, datum lines, modulation, corner treatments, upper storey step backs, windows, entrances, ground-floor design treatments, and other architectural details. Building articulation should create an active façade which helps define the public realm as an interesting and welcoming environment.

### NOT THIS



*This building is not well articulated. The facade is flat and planar. Datum lines are weak; those between ground floor and upper storeys do not have good continuity.*

### THIS



*Many historic buildings are well articulated, having distinct zones in the facade (base-middle-top), strong horizontal and vertical datum lines (columns and cornices), and an emphasis on doors and windows.*

### THIS



*Contemporary buildings often exhibit simplified forms and lines. They can be well articulated with good emphasis on windows and doors, clearly defined zones in the facade, and datum lines that create horizontal and vertical rhythm.*

## Active facade

A visually engaging architectural composition that is interesting to look at. The arrangement of windows, entrances, materials, massing and other elements exhibits variety and detail within an overall unified framework. An active facade leads the eye, and avoids uniformity and large areas of similar materials. Good transparency at ground level contributes to an active facade, where interior uses are visible to and animate the street edge.



*This building treats windows and balconies in different ways to relieve an otherwise simple composition. This design language is repeated throughout the facade.*

## Awning

A light, detachable structure of fabric or other flexible material supported from the building by a frame (fixed or retractable) to offer shelter from both the sun and rain.



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## Back of House

Uses and building elements that are typically located behind the building such as trash storage, mechanical equipment, loading bays, service entrances, hydro poles, transformers, and off-street surface parking. These uses and elements can detract from the quality of the pedestrian realm.



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## Building Orientation

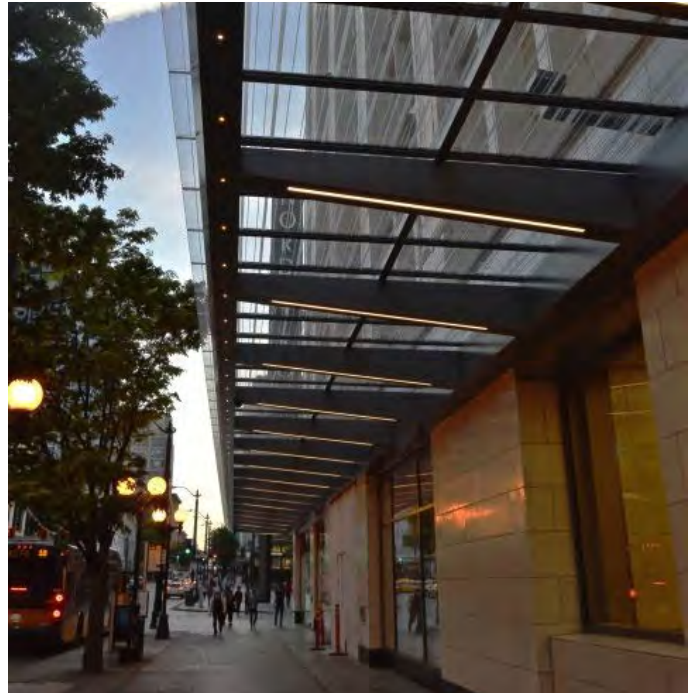
The placement of a building, its entrances and windows and the direction they are facing in relation to streets and other public open spaces. Buildings should be oriented to define and animate the public realm. Their primary entrances and facade should not be inward facing or oriented to private spaces.



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## Canopy

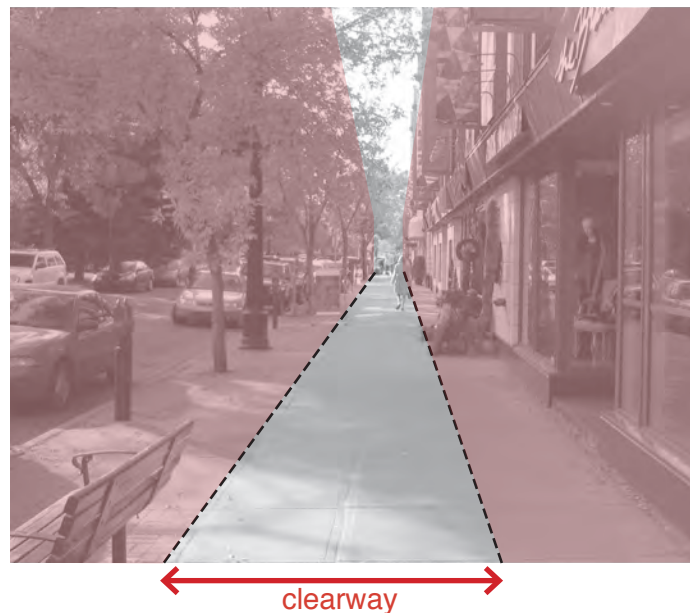
A canopy is a rigid structure extending out from the building face to provide shelter from sun and rain, and is entirely supported from the building.



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## Clearway

The pedestrian clearway is the portion of the sidewalk that has a smooth surface and is clear of obstructions in a continuous path of travel. The clearway should be unobstructed by permanent street elements such as poles, trees, benches, and overhead signs, as well as temporary elements such as patios, signs, and seasonal displays. It should be relatively straight and avoid detours around street elements such as patios.



## City Core

Prince Rupert 2030 The Vision sets out a transformative vision for how Prince Rupert will evolve. The City Core is envisioned to be comprised of the Midtown, Downtown and Marina Districts. These will be the commercial and cultural heart of the city.



*The City Core of Prince Rupert*

## Cornice

A horizontal molded that crowns, completes, or divides horizontally a building or wall, including for compositional purposes, and often reflecting the building's structure.



## Definition

The sense of enclosure created by buildings located continuously on either side of a street to create clear boundaries or "walls" that frame or define an outdoor room. A continuous planting of street trees can also be used to create street definition.



## Datum Lines

In design, a datum line is a line or plane surface that anchors other elements of the design and organizes the visual composition. Datum lines are perceived across the entire facade or massing element. They can be strong, subtle or even implied. Design elements that create datum lines include:

- columns, pilasters, cornices
- sills, headers, recesses (in repetition)
- joint lines, material changes
- the ground plane, roofline, storey divisions, massing elements
- windows, doors (in repetition)

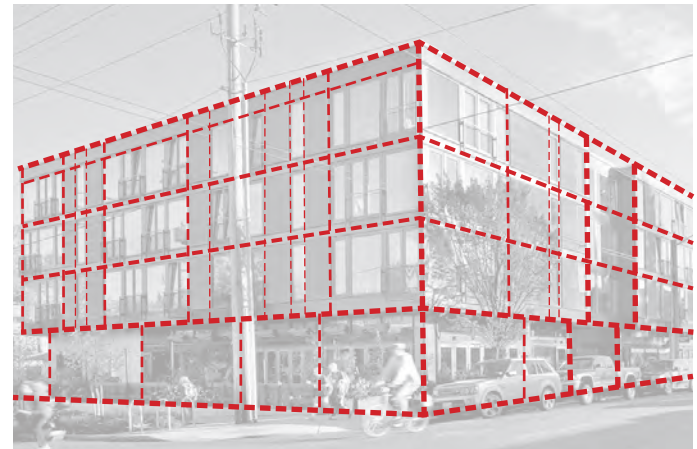
Examples of datum lines include:

**Subtle lines created by material detailing, such as these seams marking each floor level**



**Strong lines created by distinct change in materials, such as the change from red brick base to the white cladding**

**Subtle and implied lines created by the alignment of windows across different floors**



*Horizontal datum lines created by the ground plane, floor lines, and roof line. Vertical datum lines created by building corners and alignment of windows across floors.*

**Strong lines created by formal elements such as cornices and columns**



**Implied lines created by the alignment of windows and sign bands across bays in the facade**

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## Fenestration

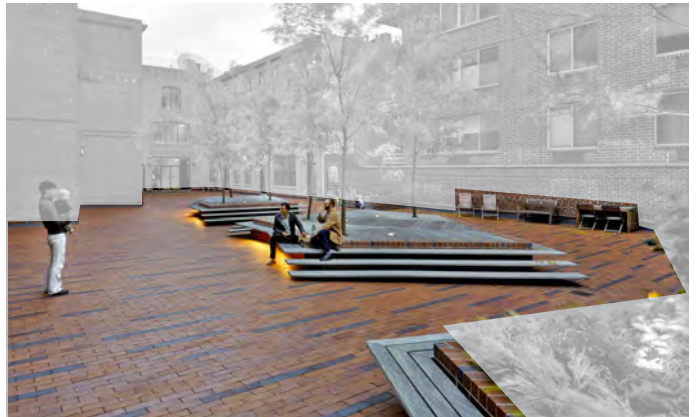
The design and disposition of windows and other exterior openings of a building.




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## Hard Landscape

Elements of the site, generally not part of the building, that are human made, such as paving, walls, fences, furniture, poles, and lighting. Contrast to soft landscaping which relates to planting.




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## Overhang

Where an upper storey(s) of a building projects farther than a lower storey(s). Where the ground floor of buildings is set back from the property line, a structural building overhang can function as rain protection for pedestrians.

overhang →



## Pedestrian Scale

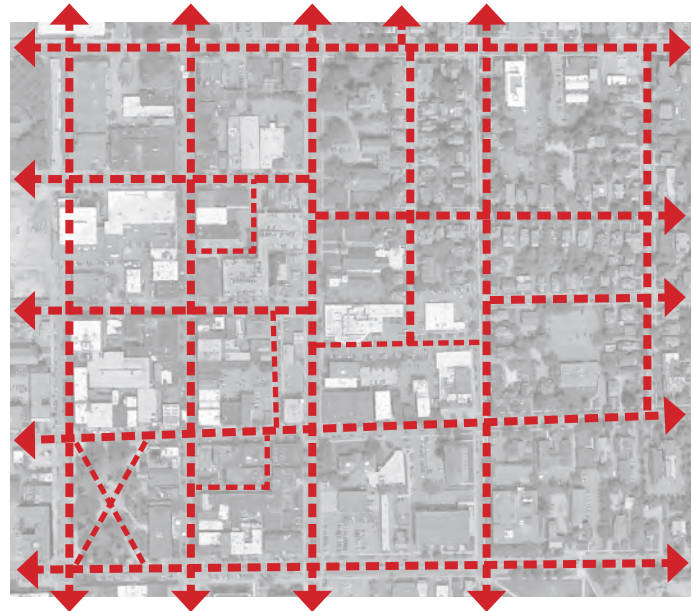
Creating cities, buildings and objects that relate to the size of people and our use of those environments and objects. Pedestrian scale is important in creating comfortable environments that people can relate to. Pedestrian scaled cities have a fine grain that is oriented to pedestrian activity and the slow speed of pedestrian movement. When environments and buildings are designed for vehicular scale, they have large spaces with few interesting details because they are designed for the fast speed of vehicular movement.



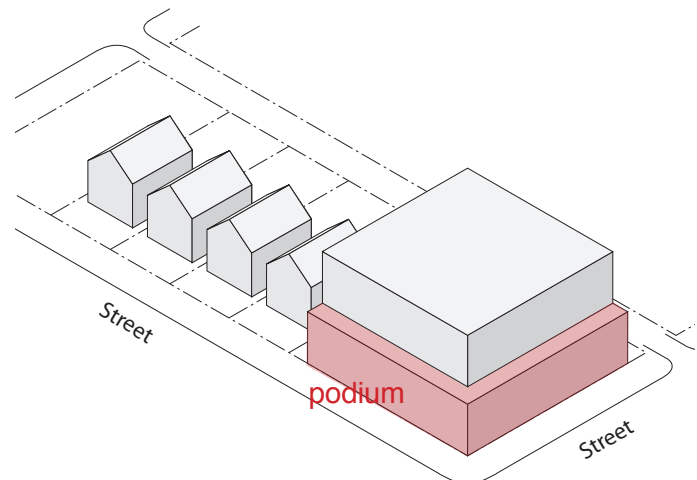
**Building scale:** pedestrian scaled buildings have a street wall that allows people on upper storeys to make eye contact with pedestrians; ground level uses that are visible on the street and invite interaction; frequent entrances; and architectural details that are in proportion to the human body.

## Podium

The lower mass of a building that creates the street wall. To create a podium, the upper storeys of the building must have been set back from the podium, and/or have a smaller floorplate.



**City scale:** pedestrian scale is many short blocks and pedestrian walkways providing an interconnected network of choices.

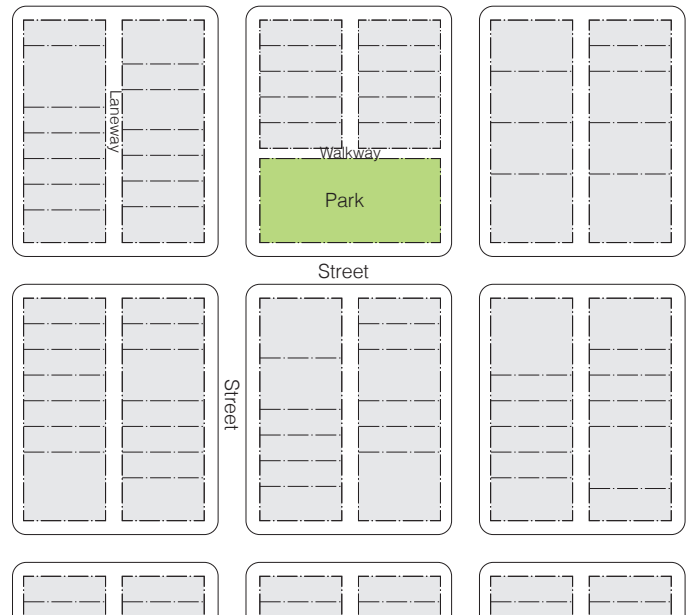


## Public Realm

The publicly accessible spaces of the city such as streets, laneways, parks, plazas and walkways. Generally the public realm is owned by the City, but it can also include privately owned and maintained open spaces that have a legal agreement that guarantees public access.

## Private Realm

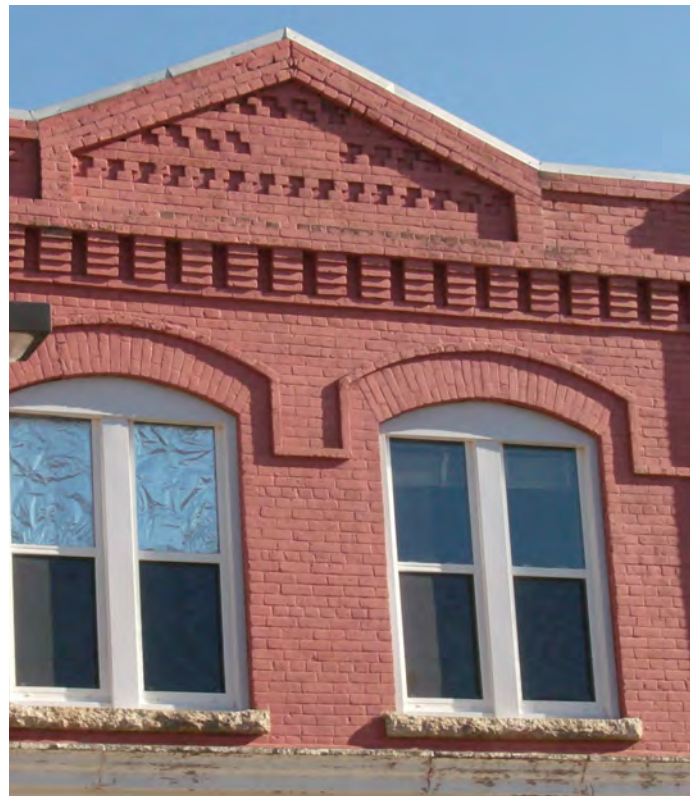
Lots, sites and properties that are owned by private individuals and companies. These City Core Development Permit Guidelines for Commercial, Industrial and Multifamily Development apply to the private realm.



The public realm includes streets, laneways, open spaces and mid-block connections (unshaded and green). The private realm is the lot fabric (grey).

## Punched Window

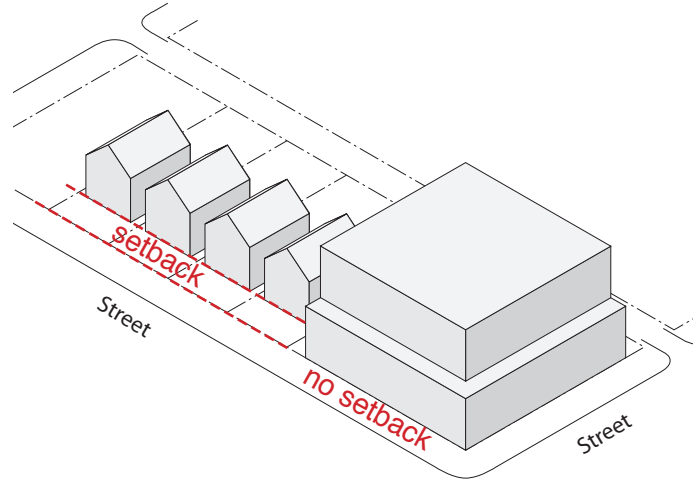
A window opening that appears to be cut out of a planar surface. Punched windows are typically recessed with a simple material return around their edges. They are common in upper storey windows of historic and traditional buildings of downtowns.



Punched windows have simple recesses (or projections) from the wall.

## Setback

The distance from the property line to the main line of the building's facade, measured along the ground.



## Sign band

A zone within the facade above the ground floor display windows for retail signs.

sign band →



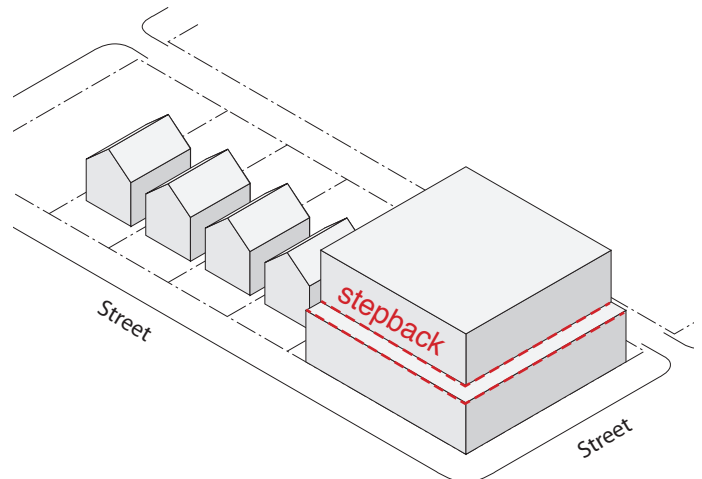
## Soft Landscape

Landform, and planting including trees, shrubs, groundcovers, and grasses.



## Stepback

The distance from the main line of the building's facade at the street wall (podium) level to the main line of the building's upper facade. The stepback is what sculpts the massing of the building to establish the podium. Stepbacks are used to ensure the street wall is of a pedestrian scale and upper storeys are farther away from the street, so that there is greater access to sunlight and sky view for pedestrians on sidewalks.



## Street Rhythm

The pattern of buildings, store fronts, entrances, and windows along a street. Historically, the rhythm of these elements was closely spaced and oriented to pedestrians, and helped to create pleasant streetscapes.



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## Street Wall

The plane created by building fronts located continuously along the street edge at the podium level, generally 2 to 4 storeys in height.



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## Streetscape

The physical configuration and visual appearance of everything within the street corridor, as perceived by pedestrians. The streetscape includes the sidewalks, street furniture, trees, road, poles, and the building faces that define its edges. Creating great streetscapes is a fundamental goal of these guidelines. The public and private realm elements that make up the streetscape should be coordinated to ensure they are pedestrian scale, easily walked, active and inviting.



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## Tuck Under Parking

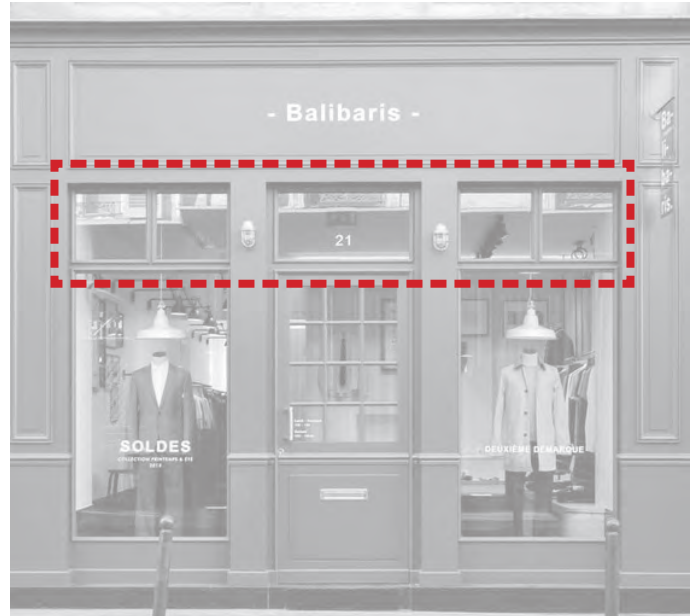
Parking located behind a building and tucked under a building overhang.



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## Transom Window

A fixed window over a door or another window dividing it into stages or heights.




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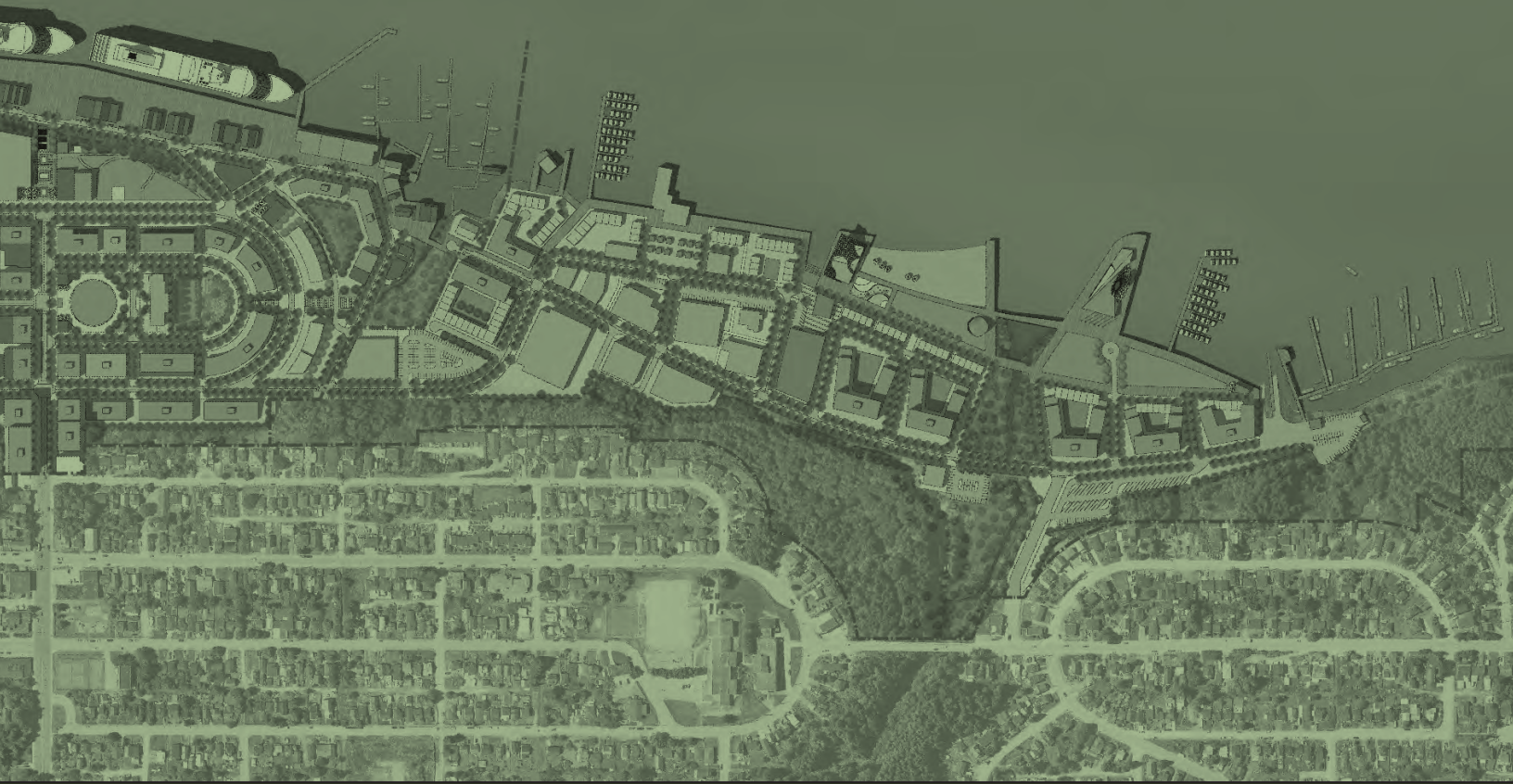
## Transparency

The placement and treatment of windows and doors within the facade. Good transparency is when interior (private) uses are visible from, and can even spill out onto, the (public) sidewalk, and when the use of public space is visible from inside buildings, to allow for casual surveillance. Transparency at ground level is important in creating vibrant streetscapes.









**CITY CORE DEVELOPMENT PERMIT GUIDELINES  
FOR COMMERCIAL, INDUSTRIAL AND MULTIFAMILY DEVELOPMENT**

CITY OF  
**PRINCE RUPERT**

# **MULTI-FAMILY AREAS**

DEVELOPMENT PERMIT  
GUIDELINES



# Guidelines

## Site Services

1. All on-site services must be underground;
2. On-site storm water management measures must be provided so that post-development hydrology (runoff and infiltration) reflects pre-development hydrology; and
3. Where collection of solid waste is proposed to be centralized, garbage and recycling compounds must be provided in safe and convenient locations and not within a front or exterior side yard.

## Building Guidelines

1. Townhouse units must not exceed a maximum of five dwelling units in a building;
2. Buildings over one storey must be massed to reduce the appearance of the height and increase ground floor weight through terracing or tapering of building mass and roof lines;
3. Building design must reflect a residential character. Sloping rooflines and extensive use of gables and dormers are encouraged;
4. To maintain a residential scale, a major break in the building form must be incorporated into the architecture at least every 15 metres;
5. Building character must convey permanence, provide visual interest at the pedestrian level, and avoid monotonous or overbearing character. Visual interest can be achieved by the use of large, robust, unique architectural details to offset otherwise uniform exterior treatments, or through the provision of rich detail in the use of wood planks, cement board planks, brick and stone combined with visually strong trim features;
6. Exterior trim and architectural features must be robust and weather resistant;
7. Where a building is adjacent to or visible from a street, its principal elevation must be oriented to the street and designed in such a manner as to promote a pedestrian-oriented, streetscape including:
  - a. the provision of a strongly articulated, primary pedestrian entrance to each ground level unit or in the case of apartments to each building, accentuated by:
    - i. door design, trim, and hardware;
    - ii. provision of a door window or sidelight;
    - iii. porch, dormer or recessing; and
    - iv. a direct hard surfaced pedestrian pathway to the street. Use of smooth, level non-slip permeable surfacing suitable for use by those with mobility impairment (e.g. walkers, wheelchairs) is also permitted.

- b. significant facade and roof articulation and emphasized by variation in exterior material textures;
  - c. differentiation of the ground level from higher stories and between buildings through variation in articulation, exterior material texture, and architectural details such as trim and window treatment; and
  - d. provision of a primary visual focal point;
8. Buildings adjacent to a street corner must be oriented to both streets and building design should add significance or prominence to the corner by providing visual and primary pedestrian access to the building from the corner;
9. Garage doors visible from an abutting street must be recessed to reduce their visual prominence and provide visual and pedestrian interest through door design, hardware and inclusion of windows;
10. Design must encourage crime prevention by using passive security design elements including:
  - a. Providing adequate pedestrian level lighting for sidewalks, pathways, and parking areas to promote safe evening use;
  - b. Designed and siting buildings to facilitate a high degree of visual surveillance of adjacent streets and onsite, outdoor common areas from unit windows;
  - c. Designing and siting entrances to facilitate a high degree of visual surveillance from unit windows, adjacent streets and internal roads; and
  - d. Locating habitable rooms with windows to face streets, sidewalks, and associated open space to promote informal surveillance through a high level of visual oversight;
11. Buildings must provide visual privacy between units and also between private amenity spaces such as balconies and patios;
12. Buildings with monotonous flush facades, flat rooflines, and a lack of ornament or features, or emphasis or architectural details shall be avoided.
13. Building shapes, sizes, heights and locations shall provide for maximum privacy, view, and sunlight for the development while respecting the view, privacy, and access to sunlight of adjacent residential uses.

#### **Site Design:**

1. Walls, fences, berms grade changes or landscaping shall not hazardously obscure the vision of pedestrian or vehicular traffic within the development site or from the street.
2. Landscaping plantings shall generally be hardy and appropriate for their respective purposes, i.e., screening, visual interest, soil stabilization, shade, windbreak, etc.
3. Hard landscaping features shall be durable, decorative and complement building

finishes.

4. Landscaping generally shall be used, in preference to fences and walls, to provide buffers and screens.
5. Site design shall contribute to a sense of neighbourhood identity and security by providing places for social engagement without creating hard barriers to adjacent streets. Site design shall include common-use open spaces that are:
  - a. Attractive and inviting;
  - b. Usable for meetings and recreation;
  - c. Secure, i.e, overlooked by on-site residential units and adjacent users; and
  - d. Landscaped, consisting of some combination of trees, shrubs, hedges, ground cover, lawns or other horticultural elements.
6. One large, isolated, treeless parking area per site shall be avoided in preference for several smaller parking areas separated by landscaping and located in rear and side yards or covered parking spaces in, under, attached or close to dwelling units.
7. Pedestrian access to important on and off-site destinations shall be provided.
8. Loading zones, garbage facilities and on-site parking areas shall be screened and/or located away from public entrances and front of building activity.

CITY OF  
**PRINCE RUPERT**

# **GENERAL COMMERCIAL AREAS**

**DEVELOPMENT PERMIT  
GUIDELINES**



# Guidelines

Development permits for commercial development outside the City Centre will be considered in accordance with the following:

## A. Form and Character

1. Buildings must have a compatible scale, form, height, setback, materials and character with neighbouring developments.
2. Building perimeters shall relate to a pedestrian scale. This includes the detailing of the façade, window size, awnings and roof canopies. Create visual interest with variations in height and massing. Awnings, lighting fixtures and other structures shall be architecturally integrated. Large areas of a single building material without architectural detail are discouraged to create visual interest and to avoid a monotonous appearance.
3. All developments must screen rooftop mechanical equipment to view or blend in with the roof and elevator penthousing. This screening shall be incorporated into the overall architectural treatment of the building.
4. Building features such as stepped or varied building massing, articulated building walls and roof lines shall be architecturally integrated to develop building form and character.
5. Building design and materials will be of a high standard to ensure permanence and quality.
6. Any building wall visible from street view shall include the same standard of finishing and material as the front of the building to ensure an attractive appearance.
7. Any site containing more than one building shall ensure that the buildings share common architectural features.

## B. Signage

1. When constructing a sign, elements like size, colors, design and placement, and supporting structures or framework shall be coordinated with the architectural elements of the buildings face and the sites surrounding signs and landscaping.
2. Permanent materials are to be used such as exposed wood, metal and flat stone.
3. Raised or recessed letters or symbols are to be used as they provide relief to signs.

4. Combined tenant signage shall be used in multi-tenant buildings.

### **C. Siting, Landscaping and Screening**

1. A Landscape Architect shall prepare a plan that incorporates plant species and quantities suitable for the project.
2. A detailed landscaping and screening plan shall be submitted with the development permit application. This plan must be drawn to scale and show the type, size and location of the proposed landscaping.
3. Parking and outdoor storage will not be permitted within the required landscape setback.
4. A landscaped buffer area of at least 2.0 metres in width and 2.0 metres in height shall be provided in order to separate parking, services or storage areas from adjacent properties.
5. A minimum 7.5 metre continuous landscape buffer will be provided along all adjacent residential properties.
6. Loading areas, garbage and recycling containers will be screened and gated to a minimum height of 2 metres by infrastructure such as buildings, a landscaping screen, solid decorative fence or a combination of the listed infrastructure.
7. Chain link fencing is only permitted when screened by landscaping. Decorative fences are strongly encouraged.
8. Developments must incorporate a sidewalk of an appropriate width along any building to a customer entrance or adjacent parking area. Planting beds for foundation landscaping shall be incorporated, where appropriate, into the design of the sidewalk along the façade of the buildings.
9. Internal pedestrian walkways must be distinguished from driving surfaces through the use of durable materials such as pavers, bricks or concrete. This enhances pedestrian safety and comfort while adding to the overall attractiveness of walkways.
10. New developments must limit their peak run off to that which the same catchment areas would have generated prior to the development land use. This will require a storm water management plan prepared by a Professional Engineer to comply with the City's stormwater management policies.
11. The City requires the following minimum depth of topsoil or amended organic soils on all landscaped areas of a property to ensure long term viability:
  - shrubs – 450 mm

- groundcover & grass – 300 mm
- trees – 300 mm around and below the root ball

#### **D. Lighting**

1. Lighting shall be designated for security and safety. Lighting infrastructure shall avoid any glare on neighboring properties, adjacent roads, or the sky.

#### **E. Parking**

1. Vehicular movement patterns shall be illustrated to ensure adequate circulation.
2. Developments should provide bicycle parking facilities at grade near the primary building entrances.

CITY OF  
**PRINCE RUPERT**

# **INDUSTRIAL AREAS**

**DEVELOPMENT PERMIT  
GUIDELINES**



## **General Guidelines:**

All new Industrial developments must:

1. be designed to be compatible or complementary with existing and future development with respect to the siting of buildings, exterior finish, design of buildings, landscaping and design of parking areas;
2. create development that is safe, user-friendly, and visually appealing;
3. ensure that architectural design and building materials are of a high standard in order to ensure a character of development that signifies quality, stability and permanence;
4. ensure that all unenclosed storage is screened from public roads;
5. provide containers for garbage collection and recycling which will be screened from view, and located in a safe and convenient location on-site; and
6. ensure that side and rear building elevations which are visible from adjacent roads or adjacent residential properties are finished and treated similarly to the front elevation.

## **Integration with the Existing Area:**

The orientation, scale, form, height, setback, materials and character of new Industrial developments are controlled by development permit areas to ensure compatibility with the surrounding community. Specifically, these controls include:

### 1. Lighting

Building and site lighting must:

- a. be sufficient to ensure pedestrian and vehicle safety; and,
- b. be designed to minimize the illumination of any adjacent residential properties;

### 2. Storm Water Management

- a. All Industrial developments shall incorporate storm water oil separator equipment, to remove oil wastes and sediments from storm water.

### 3. Form and Character

- a. Roof equipment, including mechanical equipment and vents must be enclosed, where practically suitable, by roof parapets or other forms of screening.
- b. Site elements such as storage, shipping and loading areas, utility kiosks, transformers and meters, bay doors and garbage receptacles must be screened from adjacent roads.
- c. Unenclosed storage must be sited behind buildings or in the rear of the property, and away from adjacent land uses.
- d. The use of smooth surfaced, light coloured building materials, finished, painted and/or textured “tilt-up” concrete panels, glazing, brick, baked enamel finished metal siding or baked enamel metal panels is encouraged.
- e. The use of untreated or unfinished concrete, metal, or aluminum as a final building finish is discouraged.
- f. Building elevations fronting and visible to the street are encouraged to include offices, showrooms and decorative design elements.
- g. Building elevations fronting and visible to the street that include overhead bay doors are discouraged.
- h. Walls are to be finished with cladding that is complimentary to the cladding of the building and adds interests to the look and texture of the wall and building or with a painted mural or other artwork approved by the City, and with a sufficient setback be provided to enable some articulation of the wall, when the wall is likely to be exposed to public view.

#### 4. Landscaping

Extensive parking areas must be screened by buildings, attractive planting, or low walls.

- a. Landscaping must be provided with the objective of:
  - i. providing screening to protect the privacy of occupants of adjacent properties;
  - ii. providing an effective screen at the time of planting;
  - iii. a minimum 7.5m setback, which may be landscaped, shall be provided to separate Industrial buildings or accessory buildings from adjacent land planned to remain in a single family residential use;
  - iv. providing a landscape feature which is of a type and sufficient maturity to be hardy and resistant to abuse, including vandalism; and
  - v. providing visual diversity in parking areas.

- vi. incorporating the principles of crime prevention through environmental design (CPTED).
- b. A continuous landscaping strip of at least 2.5 m (8 ft) wide must be provided along developed portions of each side of the lot which abuts a highway. This strip may be interrupted by boulevard crossings and pedestrian accesses.
- c. If fencing is provided, it must be on the inside of the landscaping strip.
- d. All adjacent municipal boulevards must be landscaped.

**Parking:**

- 1. Parking must be screened from public view and curbed to protect pedestrian paths, building and landscape areas.
- 2. All parking spaces must be delineated with painted lines and finished in a concrete or asphalt surface.
- 3. Secure storage or parking must be provided for bicycles.

**Storage: Enclosed and Unenclosed:**

- 1. Setback
  - a. Unenclosed storage is not permitted in any required front or exterior side yard setback.
- 2. Pedestrian and Vehicular Circulation
  - a. Unenclosed storage must not impede either vehicular or pedestrian traffic;
  - b. Unenclosed storage must be situated on a site in a manner that provides for safe and attractive pedestrian access between parking areas, unenclosed storage areas and the principal use building; and
  - c. Unenclosed storage must not interfere with sight lines for either pedestrian or vehicular traffic.
- 3. Screening
  - a. All unenclosed storage must be screened from adjacent roads and residential properties, either by fencing or by landscaping;
  - b. The use of temporary wire fencing is prohibited as a permanent fencing material;
  - c. Where storage containers designed and constructed for shipping are permitted and used for permanent or long term (more than 90 days) on-site storage the placement of the containers must comply with the setback requirements for accessory buildings, except when in proximity to a residential use. In those instances, storage containers should be fully screened from the view of the residential use, and situated at least

6m (20ft) from a property boundary common to a residential use, and preferably separated further by a permanent building or structure. Storage containers should also not be stacked, unless the resulting stack is unobtrusive to adjacent properties and uses.

- d. Where storage containers on general industrial zoned lands are used in support of shipping operations (such as an off-terminal site for stacking containers for transload operations, logistics activities, a flowthrough storage yard, or other similar port-related uses), such use should be screened from adjacent residential uses.

CITY OF  
**PRINCE RUPERT**

# **RIPARIAN AREAS**

DEVELOPMENT PERMIT  
GUIDELINES



## Glossary

Riparian Assessment Area means

- (a) For a stream the 30 meter strip on both sides of the stream, measured from the top of bank, (b) for a narrow ravine, a strip on both sides of the stream measured from the high water mark to a point that is 30 metres beyond the top of the ravine bank, and
- (c) for a wide ravine, a strip on both sides of the stream measured from the high water mark to a point that is 10 metres beyond the top of the ravine bank.

SPEA means streamside protection and enhancement area usually 15 meters from the waterbody. Alternative terms used in place of SPEA are 'leave strips' or 'stream buffers'.

## Guidelines

1. Before development commences on any land within the City of Prince Rupert, the following actions must be met:
  - a) Streamside protection and enhancement areas must be identified
  - b) Leave strips are protected
  - c) A development permit issuing any alteration of the land is approved

Variances to setbacks may be granted if most riparian values are preserved in accordance with a report from a qualified professional biologist.

2. The City of Prince Rupert has enacted Riparian Area Regulations (RAR) through Development Permit Areas (DPA's) established in the Official Community Plan (OCP). A RAR applies both to private land, and the private use on Crown owned land. If any land alteration occurs within 30 meters of a stream or waterbody, a development permit under the RAR development permit area is required. The most important element of a RAR development permit is the assessment report completed by a qualified professional biologist. The report should state where the building can occur and under what conditions. The report must also indicate the site specific streamside protection and enhancement area (SPEA).
3. If the development site is not under Riparian Area Regulation Assessment Area, a letter provided by the qualified environmental professional must be provided stating that further assessment is not required.
4. The City of Prince Rupert may require the provision of works, including fencing, to be constructed on the border of the SPEA. If this is required on the site, it will preserve and protect natural watercourses and features which are seen at risk of degradation.
5. When a development proposal within a SPEA is submitted, the applicant must include a site plan drawn to the scale of 1:1000 (or 1:5000 under District approval) created by a qualified professional. This site plan must include all natural features

such as watercourses (permanent and non-permanent), riparian areas and wetlands.

6. For any development that falls within a Riparian Development Permit Area, the applicant must hire a qualified professional biologist to prepare an assessment report consistent with section four of the RAR. The report must show the biologist's certification, compliance to the RAR assessment methodology, and the biologist's professional opinion that:
  - a) The proposed development will not result in any harmful alteration, disruption, or destruction of the natural features that support fish life.  
*or;*
  - b) The SPEA will be protected from effects of the development through identified measures within the report.
7. Throughout the preparation of the assessment report, the qualified professional biologist must consider any impacts of the development within the riparian assessment area and surrounding water bodies.  
The following circumstances must be considered:
  - flood hazard;
  - slope stability and erosion;
  - storm drainage;
  - stream channel migration trends;
  - trail usage;
  - tree stand integrity and hazardous trees;
  - wildlife species that support the integrity of fish habitat;
  - cumulative impacts; and
  - if the subject requires additional expertise, the qualified professional biologist must consult with other persons who hold such expertise.
8. The relocation of non-fish bearing watercourses to accommodate the proposed development must be considered. Relocation must include on site controls for infiltration, biofiltration and/or oil grease separation, and when the outlet is to an enclosed drainage system surpassing 25 meters lengthwise.
9. The applicant must follow any measures recommended or required to protect, preserve, restore or enhance the SPEA in the assessment report if they are included as conditions in the Development Permit.
10. The qualified professional biologist will professionally judge whether or not a full RAR assessment is required when assessing ditches. If the qualified professional biologist decides an assessment report is not required for provincial submission, a smaller report specifying required SPEA width and protective SPEA measures must be submitted alongside the development application.

11. New buildings on single family lots will have a setback from the SPEA at a minimum of three meters, this will provide usable yard space.
12. Variances may be permitted within the development permit for building setback within a watercourse which are otherwise required by the zoning bylaw. These variances may be granted if the qualified professional biologist can confirm that the variance will not encroach or impact the SPEA.
13. The City of Prince Rupert can require on-site enhancements to be put in place to mitigate any impact caused by the development of the lands and to enhance or preserve the values of those watercourses.

CITY OF  
**PRINCE RUPERT**

# **HAZARDOUS AREAS**

DEVELOPMENT PERMIT  
GUIDELINES



# Guidelines

1. There shall be no site disturbances on a steep slope other than those allowed by a development permit or subject to a general exemption.
2. Excluding trees that present a safety hazard, no disturbance of vegetation or movement of substrate will be allowed where there is any potential for erosion, except as allowed in a development permit.
3. Any development must be designed to avoid storm water runoff that could destabilize the slope or cause damage to neighbouring properties.
4. Removal of vegetation must be minimized to allow only for building sites, sewage disposal systems, driveways, landscaping and other permitted uses.
5. Applicants shall be required to provide a Slope Stability Plan certified by a qualified professional with relevant expertise showing how the proposed development is to be designed and constructed in order to prevent any destabilization or erosion of the slope. The Slope Stability Plan must include, but is not limited to whichever of the following factors are relevant to the proposed development:
  - a) Slope stability prior to development, identification of any areas subject to erosion, landslide, landslip, rockfall and windthrow;
  - b) Soil types, depth and conditions;
  - c) Siting of all buildings and other structures, services, driveways and parking areas;
  - d) Stream channelling and drainage systems;
  - e) Measures to safeguard neighbouring properties and structures from hazards arising from the siting, preparation of the site and construction of the proposed development;
  - f) Design of mitigation measures such as sediment traps in areas subject to destabilization during land clearing, construction and rehabilitation;
  - g) Alternative vegetation and erosion control measures; and,
  - h) Survey of tree cover and other major vegetation cover shown before and after the proposed development.